

Site Accessibility Evaluation

Americans with Disabilities Act Title II (Public Services: State and Local Government)



Black Forest Park & Ride PU18-0082

**7503 Black Forest Rd
Colorado Springs, CO
80908**

Transit Stops

*Inspection Date: 06/07/2016
Inspector: Michael Killebrew,
Anna Kangas*

Prepared By



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TO: Mountain Metropolitan Transit

FROM: Robert L. Hernandez

DATE: January 15, 2020

SUBJECT: Black Forest Park & Ride Self-Evaluation

1. On June 7, 2016, Mr. Michael Killebrew, Title II ADA/Section 504 Coordinator performed a Title II - Americans with Disabilities Act (ADA), Self-Evaluation. This evaluation was in accordance with 1991/2010 ADA Standards for Accessible Design. The facility was re-evaluated on December 26, 2019 with the assistance of Anna Kangas, Architect/Title II ADA/Section 504 Coordinator.

2. Priorities for accessibility are assigned in accordance with the ADA Checklist for Existing Facilities. The checklist follows the four priorities listed in the Department of Justice ADA Title III regulations. These priorities are equally applicable to state and local government facilities:

Priority 1 - Accessible approach and entrance

Priority 2 - Access to goods and services

Priority 3 - Access to public toilet rooms

Priority 4 - Access to other items such as water fountains and public telephones

3. Corrective actions to findings were/will be submitted for corrective action through the city maintenance work request system and Transition Plan. Note: Recommended corrections in some cases where needed will be modified when appropriate. Corrections are developed in consideration of the "Program Access" provisions of Title II, where applicable.

4. A copy of this report will be maintained in the Office of Accessibility.

City of Colorado Springs Title II ADA/Section 504 – Manager
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Atch 1. City of Colorado Springs, ADA Self-Evaluation Report

Sincerely,



Robert Hernandez

Parking

Lat: 38.9418860000, Long: -104.7019593000

Finding: 1

The van stall is missing a separate sign identifying it as a van accessible stall.

Parking space identification signs shall include the International Symbol of Accessibility. Signs identifying van parking spaces shall contain the designation "van accessible."

2010 ADAS Section 502.6

Parking space identification signs shall include the International Symbol of Accessibility complying with 703.7.2.1. Signs identifying van parking spaces shall contain the designation "van accessible." Signs shall be 60 inches (1525 mm) minimum above the finish floor or ground surface measured to the bottom of the sign.

Citation:

2010 ADAS Section: 502.6

As Built:

There is not a separate van accessible parking sign as required by City Code 7.4.205 C 3 F

Recommendation:

Install a separate van accessible parking sign under the accessible parking sign as required by City Code 7.4.205 C 3 F

Barrier Priority:

1 (High): Should be completed immediately. (Includes findings that have little or no cost, were in violation of the codes at the time of construction, or pose an imminent safety threat)



Image of noncompliant signage at accessible parking



Parking

Lat: 38.9418860000, Long: -104.7019593000

Finding: 2

There are not enough accessible stalls

There are 209 regular parking spaces and 6 accessible parking spaces. All of the accessible spaces are wide enough to qualify as van accessible. Based on 215 total parking spaces, 7 are required to be accessible, 2 of which are required to be van accessible.

2010 ADAS Section 208.2

Parking spaces complying with 502 shall be provided in accordance with Table 208.2 except as required by 208.2.1, 208.2.2, and 208.2.3. Where more than one parking facility is provided on a site, the number of accessible spaces provided on the site shall be calculated according to the number of spaces required for each parking facility.

Citation:

2010 ADAS Section: 208.2

As Built:

There are not enough accessible parking stalls.

Recommendation:

Restripe the parking and access aisles. If the parking spaces are reduced to 8'-0" wide, and all but one of the access aisles are reduced to 5'-0", there may be enough space to add another accessible parking space. However, the curb ramps may not line up with the new access aisles.

Barrier Priority:

1 (High): Should be completed immediately. (Includes findings that have little or no cost, were in violation of the codes at the time of construction, or pose an imminent safety threat)



Image of accessible parking

Total Number of Parking Spaces Provided in Parking Facility	Minimum Number of Required Accessible Parking Spaces
1 to 25	1
26 to 50	2
51 to 75	3
76 to 100	4
101 to 150	5
151 to 200	6
201 to 300	7
301 to 400	8
401 to 500	9
501 to 1000	2 percent of total
1001 and over	20, plus 1 for each 100, or fraction thereof, over 1000

Accessible Routes

Lat: 38.9418860000, Long: -104.7019593000

Finding: 3

The walking surface is obstructed. A 36" minimum clear walking surface is required. Many of these bollards have now fallen down into the ramp area which blocks more of the accessible route on the ramp face.

2010 ADAS Section 403.5.1

Except as provided in 403.5.2 and 403.5.3, the clear width of walking surfaces shall be 36 inches (915 mm) minimum.

Citation:

2010 ADAS Section: 403.5.1

As Built:

The level landing is not clear for at least 36".

Recommendation:

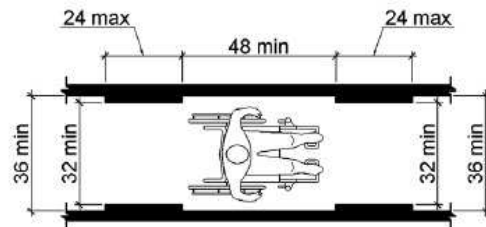
Move the bollard to provide at least 36" width.

Barrier Priority:

1 (High): Should be completed immediately. (Includes findings that have little or no cost, were in violation of the codes at the time of construction, or pose an imminent safety threat)



Image of obstructed curb ramp landing



Accessible Routes

Lat: 38.9418860000, Long: -104.7019593000

Finding: 4

There is no pedestrian access route at the street crossing.

A curb ramp, blended transition, or a combination of curb ramps and blended transitions shall connect the pedestrian access routes at each pedestrian street crossing. The curb ramp (excluding any flared sides) or blended transition shall be contained wholly within the width of the pedestrian street crossing served.

2011 PROWAG Section R207.1

A curb ramp, blended transition, or a combination of curb ramps and blended transitions complying with R304 (Curb Ramps and Blended Transitions) shall connect the pedestrian access routes at each pedestrian street crossing. The curb ramp (excluding any flared sides) or blended transition shall be contained wholly within the width of the pedestrian street crossing served.

Citation:

2011 PROWAG Section: R207.1

As Built:

There are not receiver curb ramps on the opposite sides of Woodmen and Black Forest.

Recommendation:

Install curb ramps and level landings on the opposite sides of the street to provide a safe refuge area for pedestrians. There is development in progress on the south side of the street. If nothing is planned then maybe better to take this ramp out.

Barrier Priority:

1 (High): Should be completed immediately. (Includes findings that have little or no cost, were in violation of the codes at the time of construction, or pose an imminent safety threat)



Image of lack of connecting curb ramp

Accessible Routes

Lat: 38.9418860000, Long: -104.7019593000

Finding: 5

The sidewalk damaged by vehicle.

2011 PROWAG Section R302.7.2

Vertical surface discontinuities shall be 13 mm (0.5 in) maximum. Vertical surface discontinuities between 6.4 mm (0.25 in) and 13 mm (0.5 in) shall be beveled with a slope not steeper than 50 percent. The bevel shall be applied across the entire vertical surface discontinuity. Advisory R302.7.2 Vertical Surface Discontinuities. The allowance for vertical surface discontinuities is for occasional expansion joints and objects such as utility covers, vault frames, and gratings that cannot be located in another portion of the sidewalk outside the pedestrian access route. However, objects such as utility covers, vault frames, and gratings should not be located on curb ramp runs, blended transitions, turning spaces, or gutter areas within the pedestrian access route. This may not always be possible in alterations, but should be avoided wherever possible. Vertical surface discontinuities between unit pavers should be minimized.

Citation:

2011 PROWAG Section: R302.7.2

As Built:

The new sidewalk is fractured.

Recommendation:

Replace the two panels.

Barrier Priority:

1 (High): Should be completed immediately. (Includes findings that have little or no cost, were in violation of the codes at the time of construction, or pose an imminent safety threat)

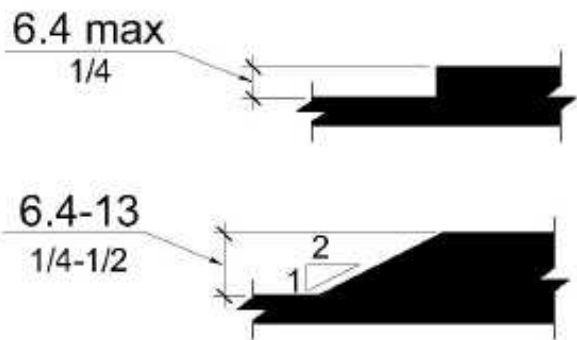


Image of fractured sidewalk