
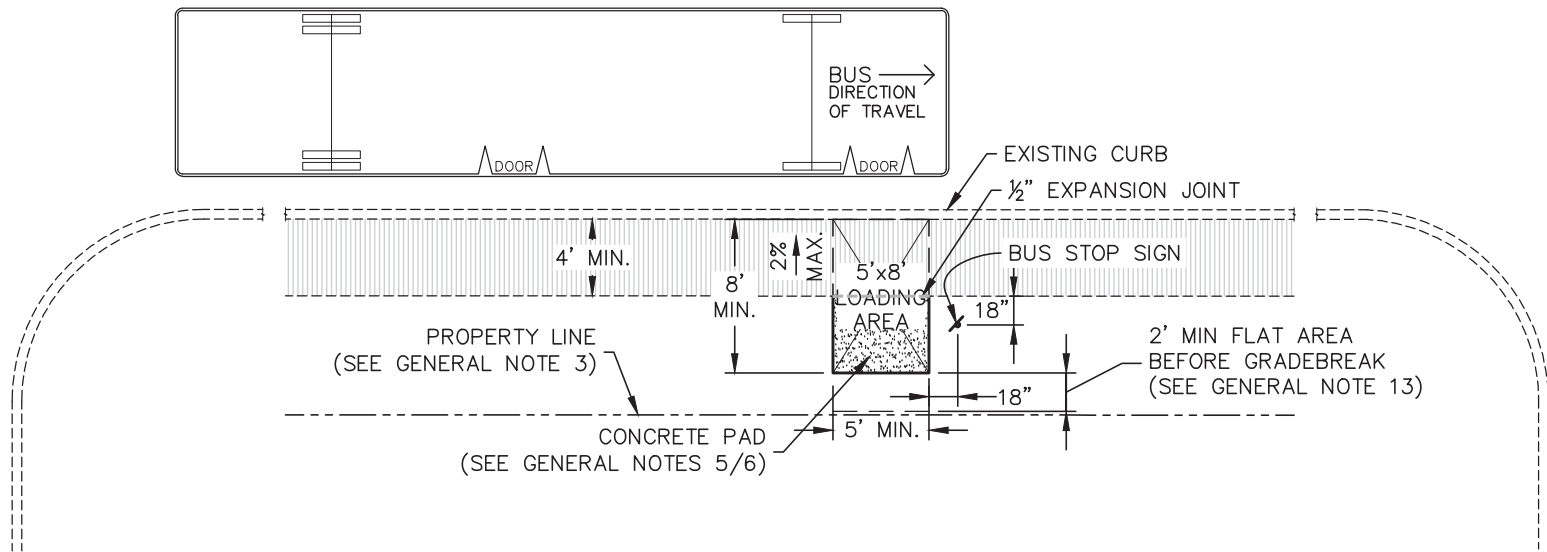


**GENERAL NOTES:**

1. ALL WORK SHALL BE DONE IN ACCORDANCE WITH CURRENT CITY OF COLORADO SPRINGS ENGINEERING DIVISION (THE CITY) STANDARD SPECIFICATIONS.
2. THE CONTRACTOR SHALL OBTAIN ALL REQUIRED PERMITS. FOR CITY PERMITS CONTRACTORS SHALL USE THE ACCELA ON-LINE PERMIT SYSTEM. ONCE CITY PERMITS ARE APPROVED AND PAID, THEN APPROPRIATE SCHEDULING AND NOTIFICATIONS SHALL BE IN ACCELA.
3. ADDITIONAL RIGHT-OF-WAY OR EASEMENT IS REQUIRED IF BUS STOP PAD EXTENDS OUTSIDE OF THE EXISTING RIGHT-OF-WAY. ALL EASEMENTS SHALL BE ACQUIRED PRIOR TO CONSTRUCTION.
4. BUS STOPS SHALL NOT BE CONSTRUCTED WITHIN THE SIGHT TRIANGLES AT INTERSECTIONS AND DRIVEWAYS AS SPECIFIED IN THE SUBDIVISION POLICY MANUAL. BUS SHELTERS, BENCHES AND SIGNAGE SHALL NOT BLOCK NOR OBSTRUCT EXISTING SIDEWALKS.
5. CURB HEAD TALLER THAN 12-INCHES SHALL BE REINFORCED WITH ASTM A 615 GRADE 60, #4 REBAR PLACED AT 18-INCHES O.C. EACH WAY. CURB HEAD TALLER THAN 30-INCHES SHALL BE ENGINEERED AND PLANS PROVIDED.
6. EXISTING UTILITIES SHALL BE LOCATED PRIOR TO THE START OF THE WORK. PRIVATE LOCATES MAY BE NEEDED TO LOCATE SERVICE LINES, CURB STOPS OR VALVES WHICH MAY NEED TO BE PROTECTED OR ADJUSTED. (CONTACT 811 FOR UTILITY LOCATION).
7. THE CONTRACTOR SHALL OBTAIN ALL REQUIRED PERMITS. FOR CITY PERMITS CONTRACTORS SHALL USE THE ACCELA ON-LINE PERMIT SYSTEM. ONCE CITY PERMITS ARE APPROVED AND PAID, THEN APPROPRIATE SCHEDULING AND NOTIFICATIONS SHALL BE IN ACCELA.
8. CONCRETE SHALL BE A CITY-APPROVED CONCRETE MIX DESIGN. CONCRETE PAD SHALL BE A MINIMUM OF 6-INCHES THICK FOR SHELTER PADS, AND A MINIMUM OF 4-INCHES THICK FOR BENCH AND LANDING PADS.
9. ALL PADS SHALL HAVE A CROSS SLOPE BETWEEN 0.5% AND 2.0%. IF A NEWLY INSTALLED BUS PAD'S CROSS SLOPE EXCEEDS 2.0% IT WILL BE REMOVED AND REPLACED AT CONTRACTOR'S EXPENSE. CROSS SLOPE WILL BE MEASURED WITH A TWO-FOOT LEVEL.
10. TOOL CONSTRUCTION JOINTS SHALL BE A MINIMUM OF 1½-INCHES DEEP AND SHALL BE AT 10-FOOT SPACING.
11. A BROOM FINISH, WITH SWEEPS PERPENDICULAR TO THE DIRECTION OF PEDESTRIAN TRAFFIC, SHALL BE APPLIED TO ALL CONCRETE SURFACES.
12. SUFFICIENT EXISTING SIDEWALK SHALL BE REMOVED AND REPLACED TO MAKE VERTICAL TRANSITIONS TO A MAXIMUM OF 15- FEET TO MATCH NEW CONCRETE PADS AND MAINTAIN A 2% MAXIMUM CROSS SLOPE. INTEGRAL CURB HEAD SHALL BE INCORPORATED TO ACCOMMODATE EXISTING GRADES AS NEEDED.
13. THE CONTRACTOR SHALL STAMP THEIR COMPANY NAME AND CONSTRUCTION DATE AT THE BACK RIGHT CORNER OF NEW BUS STOP PADS, AND AT THE BEGINNING OF NEW SIDEWALK AND CURB AND GUTTER.
14. ANY EXISTING SURFACE ADJACENT TO THE NEW BUS STOP PAD IN POOR CONDITION OR IS A TRIP HAZARD SHALL BE REMOVED AND REPLACED AS DIRECTED BY THE CITY INSPECTOR. THE EXTENTS OF THIS REMOVAL SILL BE DETERMINED BY THE CITY INSPECTOR PRIOR TO DEMOLITION.
15. A 2-FOOT WIDE (MINIMUM) LEVEL AREA SHALL BE PROVIDED WHERE LANDSCAPING ABUTS THE BUS STOP PADS. SLOPES IN SOD OR SEEDED AREAS SHALL NOT EXCEED 5:1 (HORIZONTAL TO VERTICAL).
16. CONTACT THE CITY FORESTRY DIVISION PRIOR TO DISTURBING TREES, INCLUDING ROOTS.
17. IRRIGATION SYSTEMS SHALL BE ADJUSTED AS NEEDED TO MAINTAIN OPERATION. IRRIGATION PIPING SHALL BE RELOCATED AROUND NEW CONCRETE BUS STOP PADS. IF THE IRRIGATION LINES MUST REMAIN UNDER THE BUS STOP PAD, IT SHALL BE INSTALLED IN 1½-INCH (OR ONE SIZE LARGER THAN THE IRRIGATION PIPE, WHICHEVER IS LARGER) SCHEDULE 40 P.V.C. SLEEVE EXTENDING 2- FEET BEYOND THE EDGES OF THE BUS STOP PAD. A SMALL "S" SHALL BE ETCHED INTO THE CONCRETE ABOVE THE SLEEVE AT EACH EDGE OF THE CONCRETE FOR FUTURE LOCATING.
18. ALL DISTURBED AREAS MUST BE RESTORED TO EQUAL OR BETTER CONDITION THAN EXISTING. DAMAGE TO EXISTING IMPROVEMENTS AS A RESULT OF THE CONTRACTOR SHALL BE REPAIRED/REPLACED, AT NO ADDITIONAL COST TO THE CITY, BY THE CONTRACTOR AS DIRECTED BY THE CITY INSPECTOR.
19. AN "AS-BUILT" DRAWING SHALL BE PROVIDED TO CITY ENGINEERING FOR EACH BUS SITE, INCLUDING DETAILED NOTES OF IRRIGATION AND SUBSURFACE CONSTRUCTION.



	<b>BUS STOP GENERAL NOTES</b>	
	APPROVED: <i>Jayle Sturdivant</i> City ENGINEER	
	ISSUED: 4/30/20	REVISED:



**TYPE I**  
FOR BUS STOPS WITH ATTACHED  
SIDEWALKS 4- FEET WIDE OR GREATER

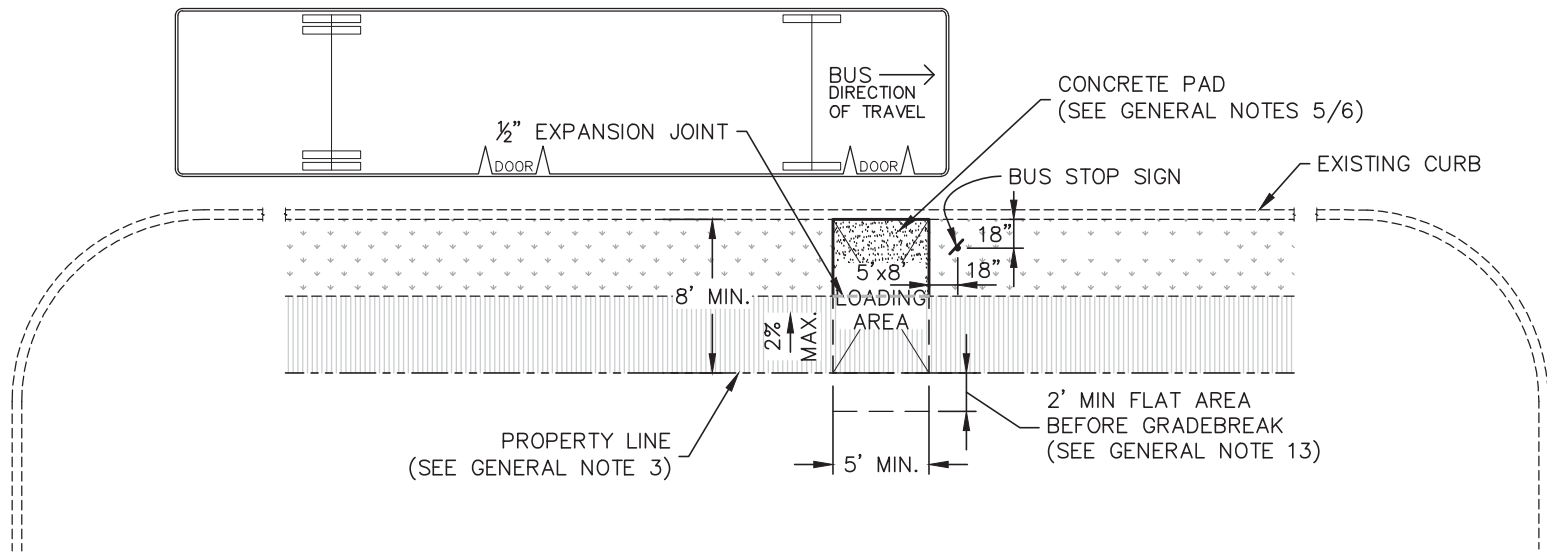
**NOTES:**

1. THE LOADING AREA SHALL BE A MINIMUM IF 5'x8'. THE EXISTING SIDEWALK MAY BE INCLUDED IN THE AREA DETERMINATION.
2. ALL DISTURBED AREAS SHALL BE RESTORED TO ORIGINAL OR BETTER CONDITION.

-  EXISTING SIDEWALK
-  NEW CONCRETE



<b>BUS STOP TYPE I</b>		
APPROVED: <i>Gayle Sturdivant</i> City ENGINEER		
ISSUED: 4/16/20	REVISED:	DRAWING NO. D-38-1



**TYPE II**  
 FOR BUS STOPS WITH DETACHED  
 SIDEWALKS 4- FEET WIDE OR GREATER

**NOTES:**

1. THE LOADING AREA SHALL BE A MINIMUM IF 5'x8'. THE EXISTING SIDEWALK MAY BE INCLUDED IN THE AREA DETERMINATION.
2. ALL DISTURBED AREAS SHALL BE RESTORED TO ORIGINAL OR BETTER CONDITION.



EXISTING SIDEWALK



NEW CONCRETE



**BUS STOP  
 TYPE II**

APPROVED:

*Gayle Sturdivant*  
 City ENGINEER

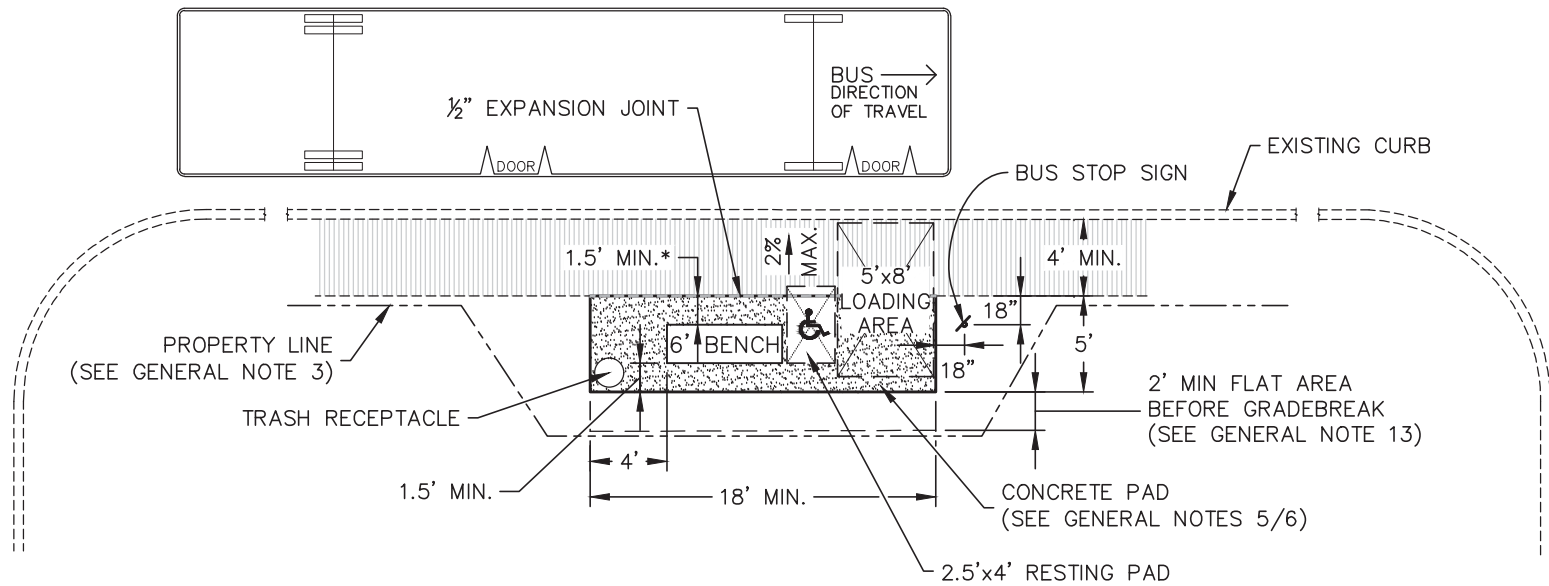
ISSUED:

4/16/20

REVISED:

DRAWING NO.



D-38-2




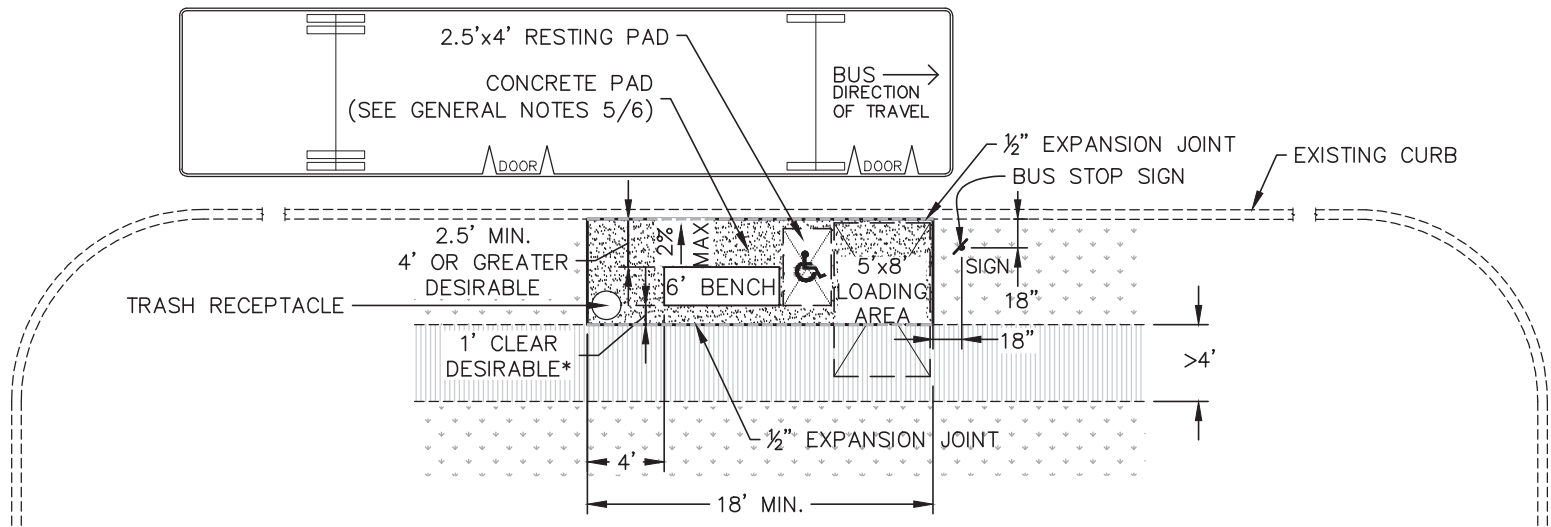
**BENCH TYPE I**  
FOR BUS STOPS WITH ATTACHED  
SIDEWALKS MIN, 4- FEET WIDE.

**NOTES:**

1. \*A 3- FEET CLEARANCE MUST BE PROVIDED BETWEEN EDGE OF SIDEWALK AND FRONT OF BENCH IF THE SIDEWALK IS CONSIDERED A MULTI-USE TRAIL.
2. THE LOADING AREA SHALL BE A MINIMUM IF 5'x8'. THE EXISTING SIDEWALK MAY BE INCLUDED IN THE AREA DETERMINATION.
3. ALL DISTURBED AREAS SHALL BE RESTORED TO ORIGINAL OR BETTER CONDITION.

-  EXISTING SIDEWALK
-  NEW CONCRETE

	<b>BUS STOP BENCH TYPE I</b>	
	APPROVED: <i>Jayle Sturdivant</i> CITY ENGINEER	
	ISSUED: 4/16/20	REVISED:



### BENCH TYPE II

FOR BUS STOPS WITH DETACHED SIDEWALKS  
AND AN AREA BETWEEN CURB AND  
SIDEWALK GREATER THAN 4- FEET WIDE.

NOTES:

1. THE LOADING AREA SHALL BE A MINIMUM IF 5'x8'. THE EXISTING SIDEWALK MAY BE INCLUDED IN THE AREA DETERMINATION.
2. ALL DISTURBED AREAS SHALL BE RESTORED TO ORIGINAL OR BETTER CONDITION.



EXISTING SIDEWALK



NEW CONCRETE



### BUS STOP BENCH TYPE II

APPROVED:

*Gayle Sturdivant*  
City ENGINEER

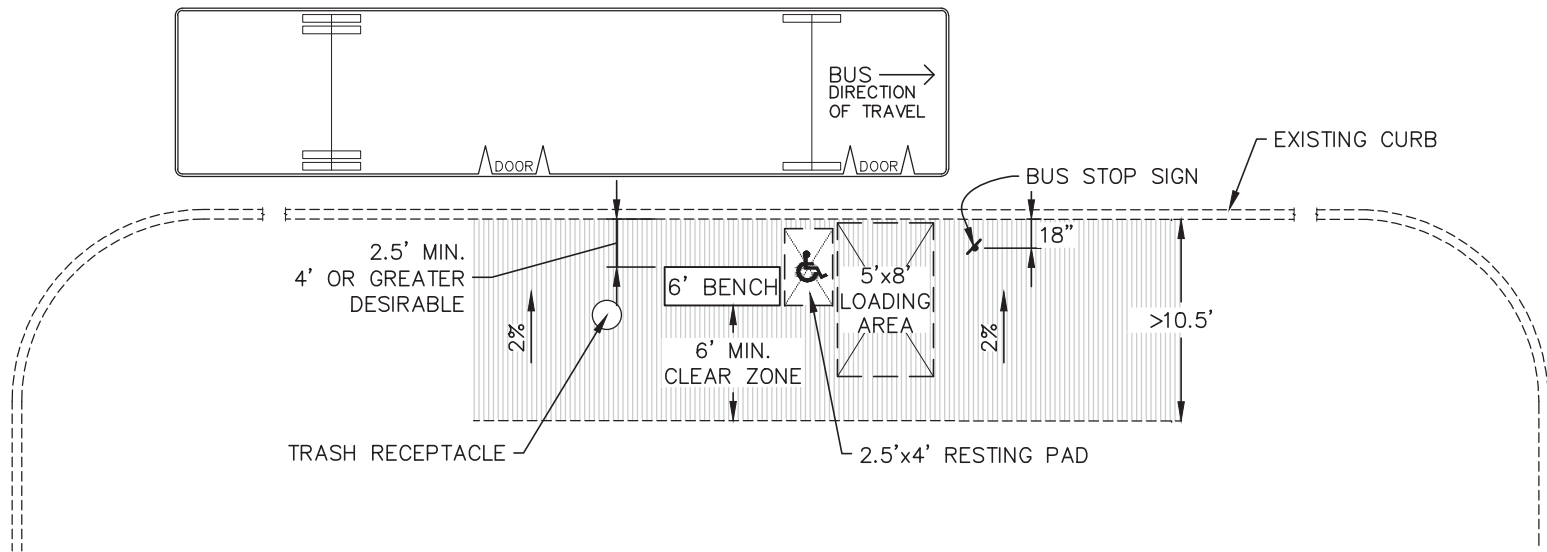
ISSUED:

4/16/20

REVISED:

DRAWING NO.

D-38-BT2



**BENCH TYPE III**  
 FOR BUS STOPS WITH ATTACHED  
 SIDEWALKS GREATER THAN 10.5- FEET

NOTES:

1. IF SIDEWALK IS CONSIDERED A MULTI-USE TRAIL, THIS DETAIL MAY NOT BE UTILIZED.
2. ALL DISTURBED AREAS SHALL BE RESTORED TO ORIGINAL OR BETTER CONDITION.

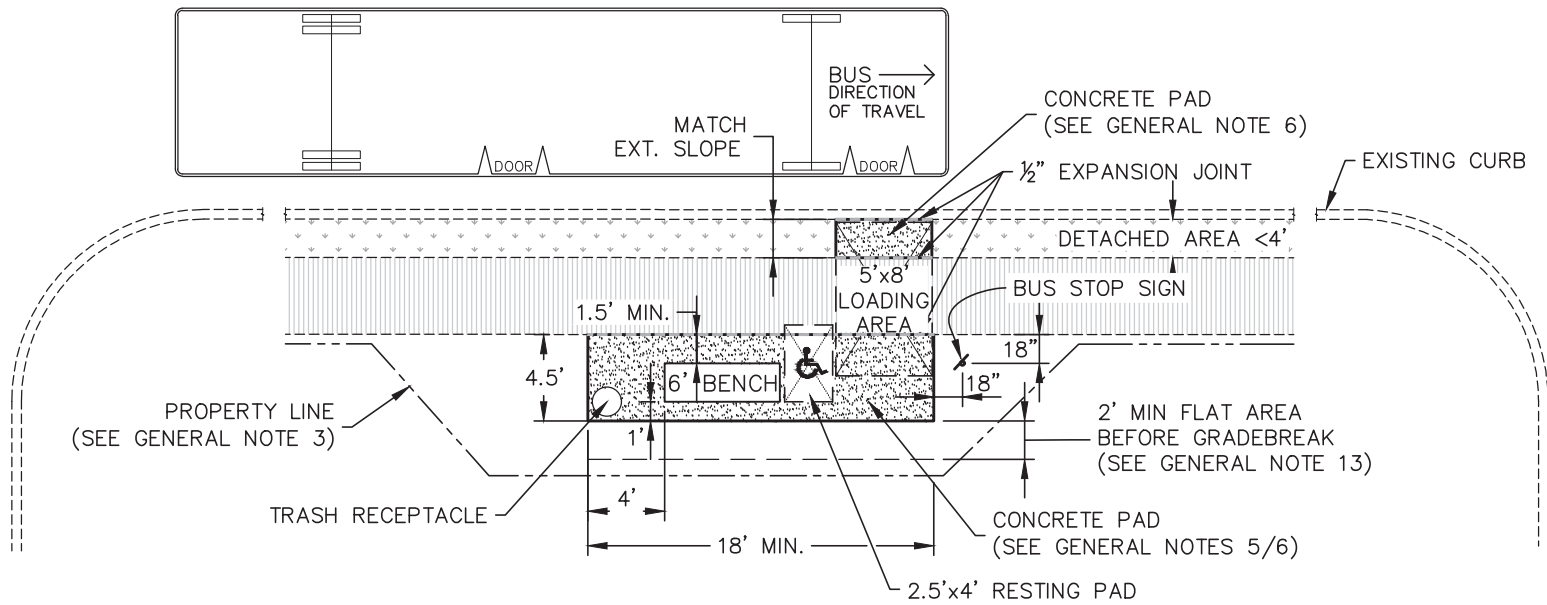
 EXISTING SIDEWALK



**BUS STOP  
 BENCH TYPE III**

APPROVED: *Jayle Sturdivant*  
 City ENGINEER

ISSUED: 4/16/20	REVISED:	DRAWING NO. D-38-BT3
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



### BENCH TYPE IV

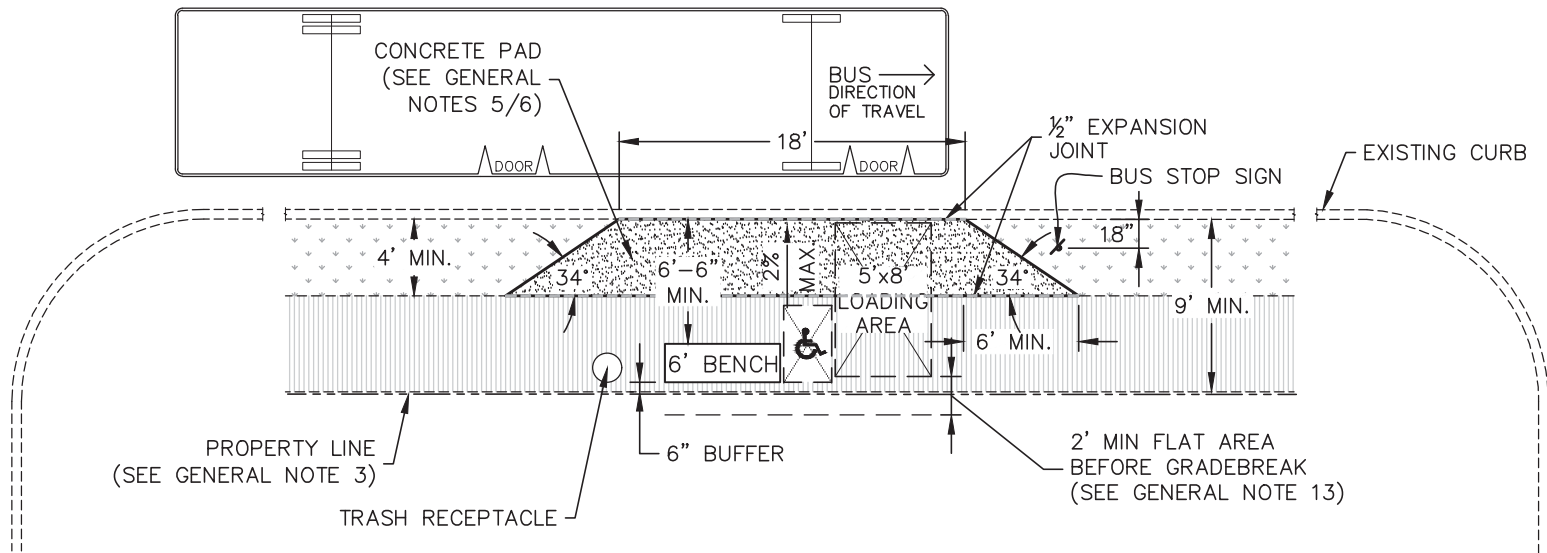
FOR BUS STOPS WITH DETACHED SIDEWALKS AND AN AREA BETWEEN WALK AND CURB IS LESS THAN 4- FEET.

**NOTES:**

1. THE LOADING AREA SHALL BE A MINIMUM IF 5'x8'. THE EXISTING SIDEWALK MAY BE INCLUDED IN THE AREA DETERMINATION.
2. ALL DISTURBED AREAS SHALL BE RESTORED TO ORIGINAL OR BETTER CONDITION.

-  EXISTING SIDEWALK
-  NEW CONCRETE

	<b>BUS STOP BENCH TYPE IV</b>	
	APPROVED: <i>Jayle Sturdivant</i> CITY ENGINEER	
	ISSUED: 4/16/20	REVISED:
		DRAWING NO. D-38-BT4





### BENCH TYPE V

FOR BUS STOPS WITH DETACHED SIDEWALKS  
AND AN AREA BETWEEN THE CURB AND  
SIDEWALK IS GREATER THAN 4- FEET WIDE

**NOTES:**

1. MAY USE BENCH TYPE II AS AN ALTERNATIVE.
2. ALL DISTURBED AREAS SHALL BE RESTORED TO ORIGINAL OR BETTER CONDITION.

-  EXISTING SIDEWALK
-  NEW CONCRETE

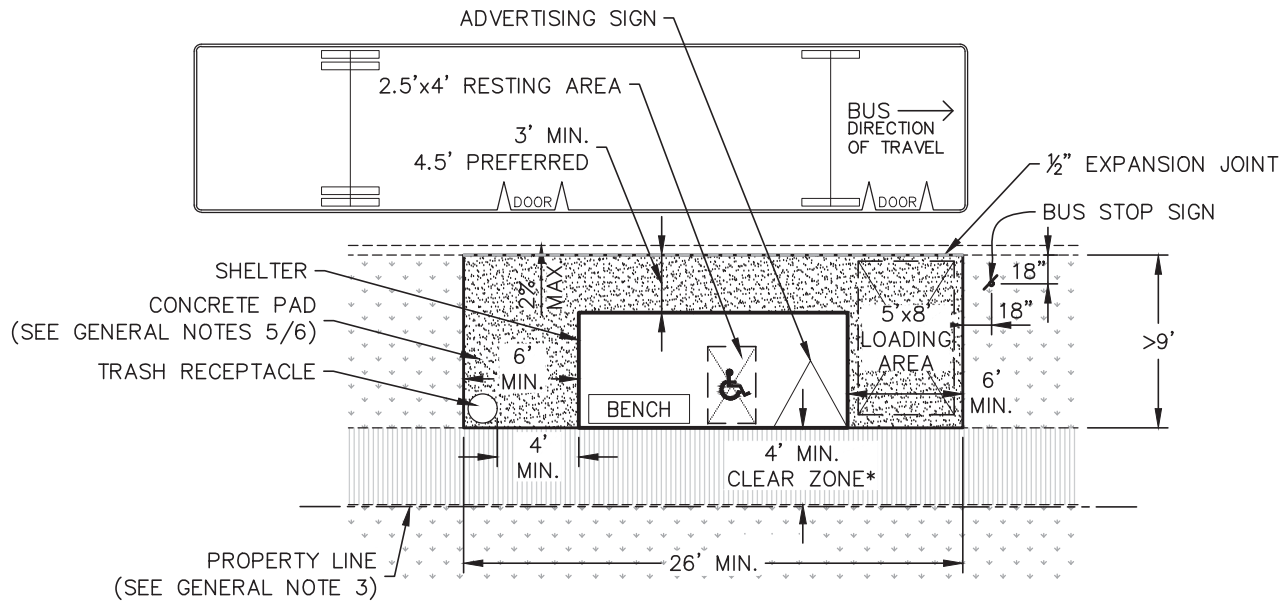


### BUS STOP BENCH TYPE V

APPROVED: *Jayle Sturdivant*  
City ENGINEER

ISSUED: 4/16/20	REVISED:	DRAWING NO. D-38-BT5
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



### SHELTER TYPE I

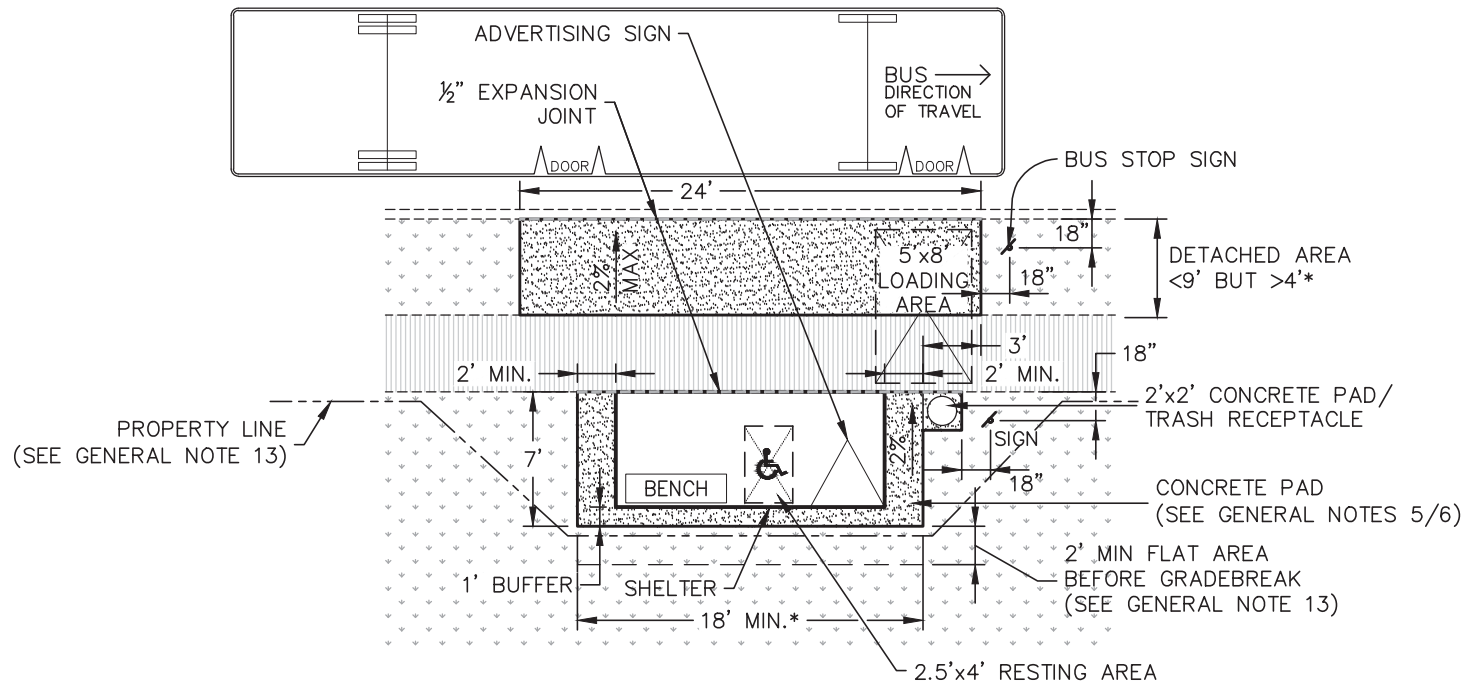
FOR BUS STOPS WITH DETACHED SIDEWALKS AND AN AREA BETWEEN CURB AND SIDEWALK GREATER THAN 9- FEET WIDE.

**NOTES:**

1. 4- FEET MINIMUM CLEAR ZONE OR MATCH EXISTING SIDEWALK WIDTH, WHICHEVER IS GREATER.
2. IF SIDEWALK IS CONSIDERED A MULTI-USE TRAIL, THIS DETAIL MAY NOT BE UTILIZED.
3. ALL DISTURBED AREAS SHALL BE RESTORED TO ORIGINAL OR BETTER CONDITION.

-  EXISTING SIDEWALK
-  NEW CONCRETE

	<b>SHELTER BUS STOP LAYOUT TYPE I</b>	
	APPROVED: <i>Gayle Sturdivant</i> <small>City ENGINEER</small>	
	ISSUED: 4/16/20	REVISED:



### SHELTER TYPE II

FOR BUS STOPS WITH DETACHED SIDEWALKS AND AN AREA BETWEEN CURB AND SIDEWALK LESS THAN 9- FEET WIDE BUT GREATER THAN 4- FEET WIDE.

**NOTES:**

1. \*IF SIDEWALK IS DETACHED LESS THAN 4 FEET USE TYPE III AND SHELTER PAD SHALL BE A MINIMUM OF 26' WIDE.
2. ALL DISTURBED AREAS SHALL BE RESTORED TO ORIGINAL OR BETTER CONDITION.



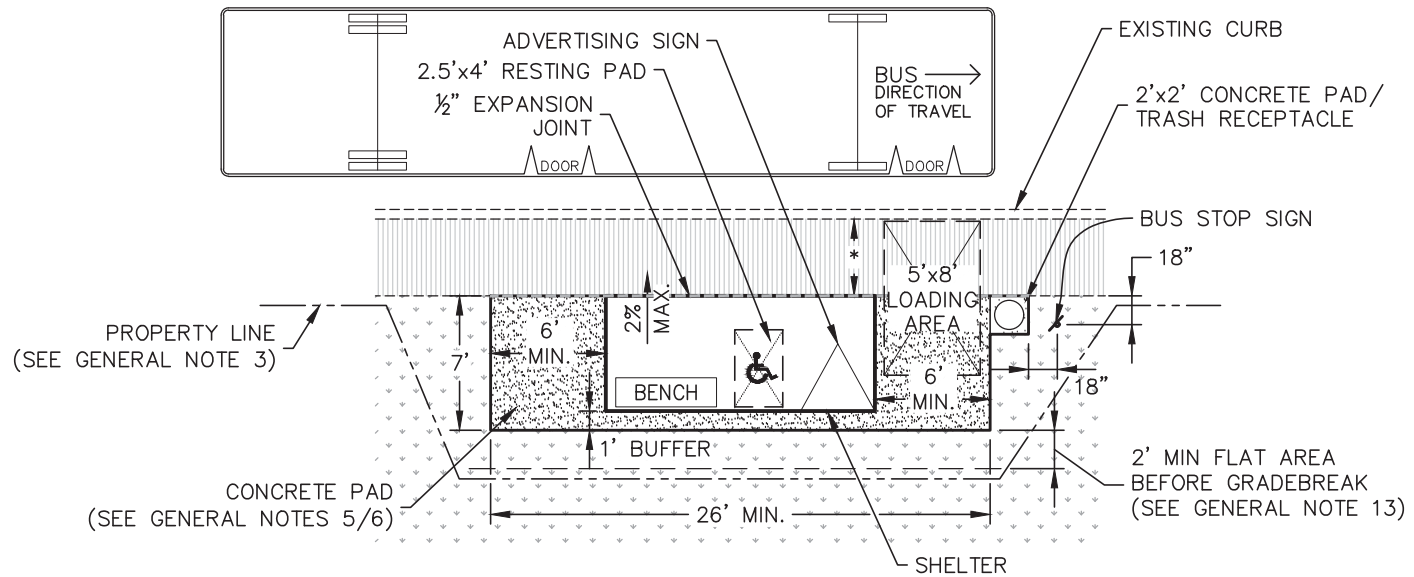
EXISTING SIDEWALK

NEW CONCRETE



### SHELTER BUS STOP LAYOUT TYPE II

APPROVED: <i>Gayle Sturdivant</i> CITY ENGINEER		
ISSUED: 4/16/20	REVISED:	DRAWING NO. D-38-ST2






### SHELTER TYPE III

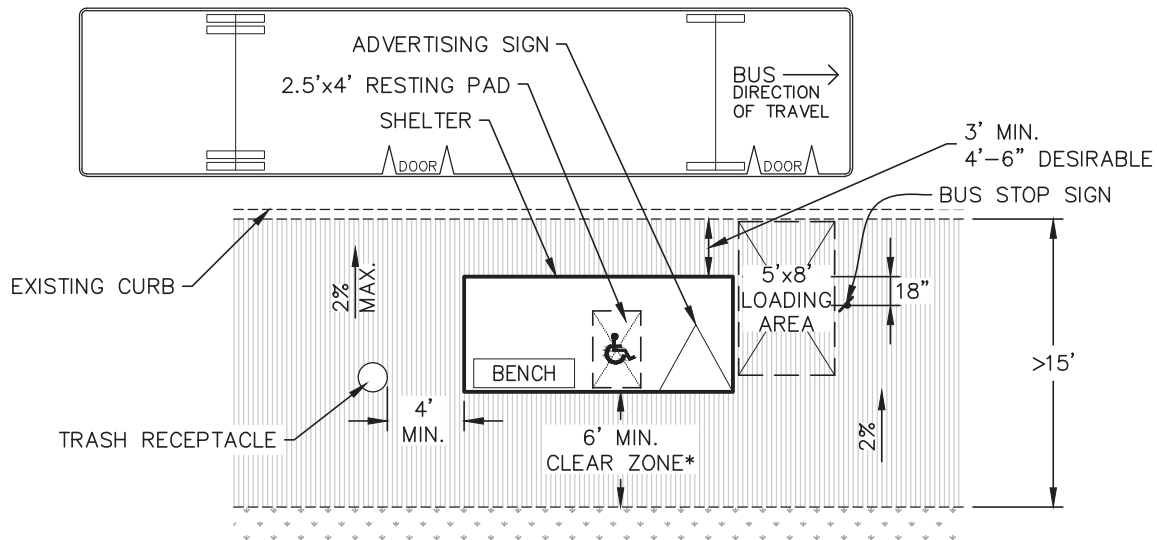
FOR BUS STOPS WITH ATTACHED SIDEWALKS.

**NOTES:**

1. \*MATCH EXISTING SIDEWALK WIDTH.
2. ALL DISTURBED AREAS SHALL BE RESTORED TO ORIGINAL OR BETTER CONDITION.

-  EXISTING SIDEWALK
-  NEW CONCRETE

	<b>SHELTER BUS STOP LAYOUT TYPE III</b>	
	APPROVED: <i>Gayle Sturdivant</i> City ENGINEER	
	ISSUED: 4/16/20	REVISED:



**SHELTER TYPE IV**  
 FOR BUS STOPS WITH ATTACHED  
 SIDEWALKS GREATER THAN 15- FEET WIDE

NOTES:

1. THIS MAY ONLY BE USED WHEN THE SIDEWALK IS NOT CONSIDERED A MULTI-USE TRAIL.
2. ALL DISTURBED AREAS SHALL BE RESTORED TO ORIGINAL OR BETTER CONDITION.



EXISTING SIDEWALK



**SHELTER BUS STOP  
 LAYOUT TYPE IV**

APPROVED:

*Gayle Sturdivant*  
 City ENGINEER

ISSUED:

4/16/20

REVISED:

DRAWING NO.

D-38-ST4