

Site Accessibility Evaluation

Americans with Disabilities Act Title II (Public Services: State and Local Government)



**Citadel Transfer Station
PU18-0081**

**780 Citadel Dr W
East Colorado Springs, CO
80909**

Transit Stops

*Inspection Date: 06/16/2016
Inspector: Michael Killebrew,
Anna Kangas*

Prepared By



(719) 385 - 5175

<https://coloradosprings.gov/>

TO: Mountain Metropolitan Transit

FROM: Robert L. Hernandez

DATE: January 15, 2020

SUBJECT: Citadel Transfer Station Self-Evaluation

1. On June 16, 2016, Mr. Michael Killebrew, Title II ADA/Section 504 Coordinator performed a Title II - Americans with Disabilities Act (ADA), Self-Evaluation. This evaluation was in accordance with 1991/2010 ADA Standards for Accessible Design. The facility was re-evaluated on December 26, 2019 with the assistance of Anna Kangas, Architect/Title II ADA/Section 504 Coordinator.

2. Priorities for accessibility are assigned in accordance with the ADA Checklist for Existing Facilities. The checklist follows the four priorities listed in the Department of Justice ADA Title III regulations. These priorities are equally applicable to state and local government facilities:

Priority 1 - Accessible approach and entrance

Priority 2 - Access to goods and services

Priority 3 - Access to public toilet rooms

Priority 4 - Access to other items such as water fountains and public telephones

3. Corrective actions to findings were/will be submitted for corrective action through the city maintenance work request system and Transition Plan. Note: Recommended corrections in some cases where needed will be modified when appropriate. Corrections are developed in consideration of the "Program Access" provisions of Title II, where applicable.

4. A copy of this report will be maintained in the Office of Accessibility.

City of Colorado Springs Title II ADA/Section 504 – Manager
30 S. Nevada Ave, Suite 301
Colorado Springs, CO 80903
robert.hernandez@coloradosprings.gov

Atch 1. City of Colorado Springs, ADA Self-Evaluation Report

Sincerely,



Robert Hernandez

Exterior - Accessible Routes

Lat: 38.8427317000, Long: -104.7660394000

Finding: 1

There is no accessible route of travel from the public way. This is a primary entrance point to the Transfer Station.

At least one accessible route shall be provided within the site from accessible parking spaces and accessible passenger loading zones; public streets and sidewalks; and public transportation stops to the accessible building or facility they serve.

2010 ADAS Section 206.2.1

At least one accessible route shall be provided within the site from accessible parking spaces and accessible passenger loading zones; public streets and sidewalks; and public transportation stops to the accessible building or facility entrance they serve.

Citation:

2010 ADAS Section: 206.2.1

As Built:

There is not a connecting ramp at Chelton and W. Portal.

Recommendation:

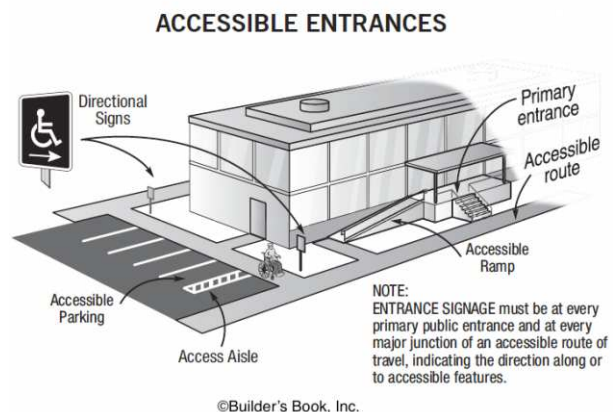
Install a curb ramp at this corner.

Barrier Priority:

1 (High): Should be completed immediately. (Includes findings that have little or no cost, were in violation of the codes at the time of construction, or pose an imminent safety threat)



Image of curb head without curb ramp



Exterior - Accessible Routes

Lat: 38.8427317000, Long: -104.7660394000

Finding: 2

There is no accessible route of travel from the public way.

At least one accessible route shall be provided within the site from accessible parking spaces and accessible passenger loading zones; public streets and sidewalks; and public transportation stops to the accessible building or facility they serve.

2010 ADAS Section 206.2.1

At least one accessible route shall be provided within the site from accessible parking spaces and accessible passenger loading zones; public streets and sidewalks; and public transportation stops to the accessible building or facility entrance they serve.

Citation:

2010 ADAS Section: 206.2.1

As Built:

The sidewalk abruptly ends near Chelton Rd and W. Portal.

Recommendation:

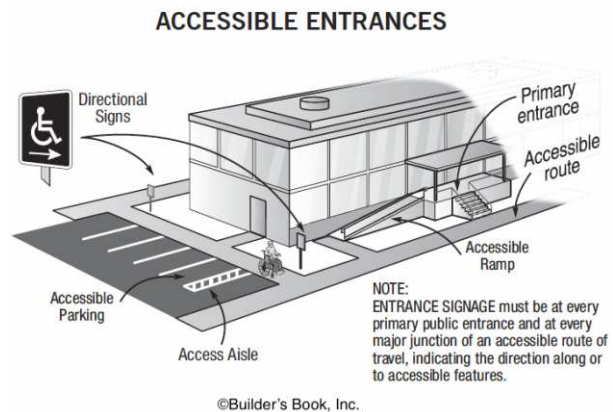
Probably nothing at this time. Consider sidewalk construction.

Barrier Priority:

1 (High): Should be completed immediately. (Includes findings that have little or no cost, were in violation of the codes at the time of construction, or pose an imminent safety threat)



Image of sidewalk abruptly ending



Exterior - Accessible Routes

Lat: 38.8427317000, Long: -104.7660394000

Finding: 3

There are multiple issues with the ramp, including:

There is no edge protection at the edges of the ramp.

The bottom landing of the ramp has 4.5% cross slope.

The middle level landing has 3.3% running slope.

The handrails do not extend the required distances beyond the top and bottom of the ramp. The handrail at the top of the ramp is not anchored, and the vertical supports lift out of the hole.

*2010 ADAS Section 405
Ramps*

Citation:

2010 ADAS Section: 405

As Built:

There are multiple issues with the ramp from the sidewalk on Chelton Rd.

Recommendation:

Add a bottom rail that restricts the opening at the bottom of the handrail to 4" maximum.

Remove and replace the bottom landing to provide maximum 2% cross slope.

Replace the middle landing and one of the ramp runs to provide the allowable running slope.

Extend the handrails the proper distances at the top and bottom of the ramp.

Barrier Priority:

1 (High): Should be completed immediately. (Includes findings that have little or no cost, were in violation of the codes at the time of construction, or pose an imminent safety threat)

Finding #3 Continued



Image of noncompliant ramp

Finding #3 Additional Finding Photos



Additional image of noncompliant ramp



Additional image of noncompliant ramp

Exterior - Accessible Routes

Lat: 38.8427317000, Long: -104.7660394000

Finding: 4

There are multiple issues with this curb ramp including:

The brick paver detectable warnings are sinking, causing up to 1/2" vertical displacement.

The side flare closest to the bike rack has a 24% slope.

*2010 ADAS Section 406
Curb Ramps*

Citation:

2010 ADAS Section: 406

As Built:

There are multiple issues with this curb ramp by the bike rack along Citadel Dr W.

Recommendation:

Remove and replace the curb ramp.

Barrier Priority:

1 (High): Should be completed immediately. (Includes findings that have little or no cost, were in violation of the codes at the time of construction, or pose an imminent safety threat)



Image of noncompliant curb ramp

Exterior - Accessible Routes

Lat: 38.8427317000, Long: -104.7660394000

Finding: 5

The surface of the pedestrian access route has vertical discontinuities greater than 1/2 inch or between 1/4 inch and 1/2 inch that are not beveled.

2011 PROWAG Section R302.7.2

Vertical surface discontinuities shall be 13 mm (0.5 in) maximum. Vertical surface discontinuities between 6.4 mm (0.25 in) and 13 mm (0.5 in) shall be beveled with a slope not steeper than 50 percent. The bevel shall be applied across the entire vertical surface discontinuity. Advisory R302.7.2 Vertical Surface Discontinuities. The allowance for vertical surface discontinuities is for occasional expansion joints and objects such as utility covers, vault frames, and gratings that cannot be located in another portion of the sidewalk outside the pedestrian access route. However, objects such as utility covers, vault frames, and gratings should not be located on curb ramp runs, blended transitions, turning spaces, or gutter areas within the pedestrian access route. This may not always be possible in alterations, but should be avoided wherever possible. Vertical surface discontinuities between unit pavers should be minimized.

Citation:

2011 PROWAG Section: R302.7.2

As Built:

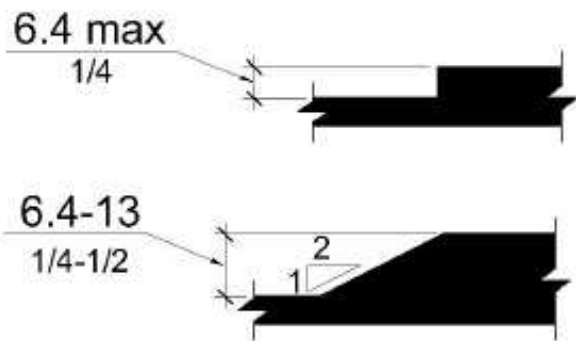
There is 1-1/2" vertical displacement in the sidewalk at the storm inlet.

Recommendation:

Remove and replace the sidewalk panels around the storm inlet.

Barrier Priority:

1 (High): Should be completed immediately. (Includes findings that have little or no cost, were in violation of the codes at the time of construction, or pose an imminent safety threat)



Exterior - Accessible Routes

Lat: 38.8427317000, Long: -104.7660394000

Finding: 6

The surface of the pedestrian access route is not firm, stable and slip resistant. In addition, the asphalt at the base of the curb ramp is deteriorated, with a hole 2" deep by 16" by 12".

The surfaces of pedestrian access routes and elements and spaces along the pedestrian access route that are required to be accessible that connect to pedestrian access routes shall be firm, stable, and slip resistant.

2011 PROWAG Section R302.7

The surfaces of pedestrian access routes and elements and spaces required to comply with R302.7 that connect to pedestrian access routes shall be firm, stable, and slip resistant and shall comply with R302.7.

Citation:

2011 PROWAG Section: R302.7

As Built:

There is considerable debris build up in the curb ramp at Citadel Dr W.

Recommendation:

Remove and replace the asphalt at the base of the curb ramp. Keep the curb ramp clear of debris.

Barrier Priority:

1 (High): Should be completed immediately. (Includes findings that have little or no cost, were in violation of the codes at the time of construction, or pose an imminent safety threat)



Exterior - Parking

Lat: 38.8427317000, Long: -104.7660394000

Finding: 7

The required parking signage is missing.

Parking space identification signs shall include the International Symbol of Accessibility (ISA). Signs identifying van parking spaces shall contain the designation "van accessible." All signs shall be 60 inches minimum above the finish floor or ground surface measured to the bottom of the sign.

2010 ADAS Section 502.6

Parking space identification signs shall include the International Symbol of Accessibility complying with 703.7.2.1. Signs identifying van parking spaces shall contain the designation "van accessible." Signs shall be 60 inches (1525 mm) minimum above the finish floor or ground surface measured to the bottom of the sign.

Citation:

2010 ADAS Section: 502.6

As Built:

Accessible parking signage is missing on two of the accessible parking spaces.

Recommendation:

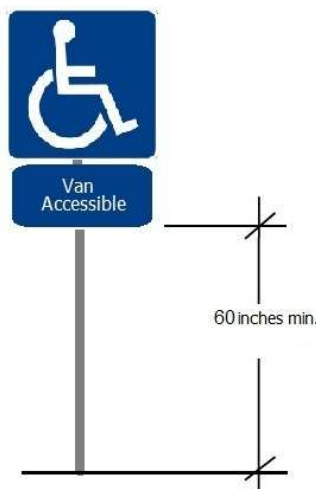
Install accessible parking signage.

Barrier Priority:

1 (High): Should be completed immediately. (Includes findings that have little or no cost, were in violation of the codes at the time of construction, or pose an imminent safety threat)



Image of accessible parking spaces without signage



Exterior - Parking

Lat: 38.8427317000, Long: -104.7660394000

Finding: 8

The required parking signage is missing.

Parking space identification signs shall include the International Symbol of Accessibility (ISA). Signs identifying van parking spaces shall contain the designation "van accessible." All signs shall be 60 inches minimum above the finish floor or ground surface measured to the bottom of the sign.

At the follow-up visit, the parking curb had been removed from this parking space.

2010 ADAS Section 502.6

Parking space identification signs shall include the International Symbol of Accessibility complying with 703.7.2.1. Signs identifying van parking spaces shall contain the designation "van accessible." Signs shall be 60 inches (1525 mm) minimum above the finish floor or ground surface measured to the bottom of the sign.

Citation:

2010 ADAS Section: 502.6

As Built:

One of the parking spaces has a post but no sign.

Recommendation:

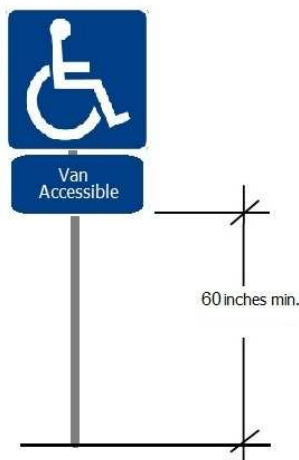
Install an accessible parking sign.

Barrier Priority:

1 (High): Should be completed immediately. (Includes findings that have little or no cost, were in violation of the codes at the time of construction, or pose an imminent safety threat)



Image of accessible parking without signage



Exterior - Parking

Lat: 38.8427317000, Long: -104.7660394000

Finding: 9

The striping and markings for the accessible parking stall, loading/unloading access aisle are dilapidated and in need of repair and/or maintenance.

*2010 ADAS Section 502.3.3
Access aisles shall be marked so as to discourage parking in them.*

Citation:

2010 ADAS Section: 502.3.3

As Built:

The striping of the parking spaces and access aisles is faded.

Recommendation:

Restripe the parking spaces and access aisles.

Barrier Priority:

1 (High): Should be completed immediately. (Includes findings that have little or no cost, were in violation of the codes at the time of construction, or pose an imminent safety threat)



Image of faded accessible parking striping

Finding #9 Additional Finding Photos



Additional image of faded markings on the parking spaces

Exterior - Parking

Lat: 38.8427317000, Long: -104.7660394000

Finding: 10

The access aisle is not a minimum 5 feet wide.

A accessible parking stall's access aisle must be a minimum of 5 feet wide measured from centerline to centerline.

Where the access aisle is not adjacent to another parking space or access aisle, measurements shall be permitted to include the full width of the line defining the access aisle.

2010 ADAS Section 502.3.1

Access aisles serving car and van parking spaces shall be 60 inches (1525 mm) wide minimum.

Citation:

2010 ADAS Section: 502.3.1

As Built:

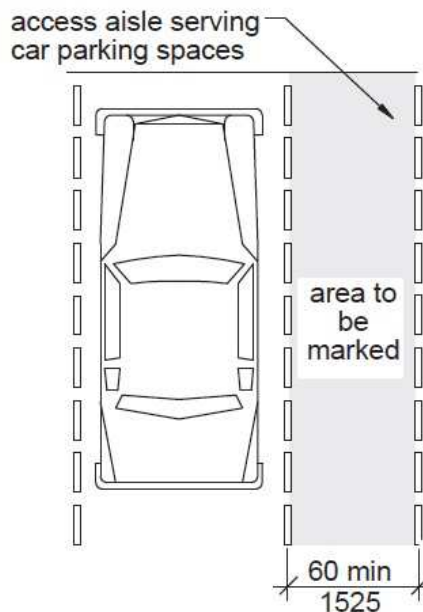
One of the access aisles is 4'-6" wide.

Recommendation:

Restripe the parking and access aisle to provide a 5'-0" wide access aisle with 8'-0" wide parking space.

Barrier Priority:

1 (High): Should be completed immediately. (Includes findings that have little or no cost, were in violation of the codes at the time of construction, or pose an imminent safety threat)



Exterior - Parking

Lat: 38.8427317000, Long: -104.7660394000

Finding: 11

The surfaces of the accessible parking spaces and access aisles are not firm, stable, and slip resistant.

Floor and ground surfaces shall be stable, firm, and slip resistant

Parking spaces and access aisles serving them shall comply with 302. Access aisles shall be at the same level as the parking spaces they serve. Changes in level are not permitted.

2010 ADAS Section 502.4

Parking spaces and access aisles serving them shall comply with 302. Access aisles shall be at the same level as the parking spaces they serve. Changes in level are not permitted.

2010 ADAS Section 302.1

Floor and ground surfaces shall be stable, firm, and slip resistant and shall comply with 302.

Citation:

2010 ADAS Section: 502.4, 302.1

As Built:

The pavement in the far right accessible space is deteriorating.

Recommendation:

Remove and replace the pavement.

Barrier Priority:

1 (High): Should be completed immediately. (Includes findings that have little or no cost, were in violation of the codes at the time of construction, or pose an imminent safety threat)



Image of deteriorated pavement in the accessible parking space

Exterior - Parking

Lat: 38.8427317000, Long: -104.7660394000

Finding: 12

The van stall signage is faded.

Parking space identification signs shall include the International Symbol of Accessibility. Signs identifying van parking spaces shall contain the designation "van accessible."

2010 ADAS Section 502.6

Parking space identification signs shall include the International Symbol of Accessibility complying with 703.7.2.1. Signs identifying van parking spaces shall contain the designation "van accessible." Signs shall be 60 inches (1525 mm) minimum above the finish floor or ground surface measured to the bottom of the sign.

Citation:

2010 ADAS Section: 502.6

As Built:

The van accessible parking sign is faded.

Recommendation:

Replace the van accessible parking sign.

Barrier Priority:

1 (High): Should be completed immediately. (Includes findings that have little or no cost, were in violation of the codes at the time of construction, or pose an imminent safety threat)



Image of faded van accessible signage

