

Site Accessibility Evaluation

Americans with Disabilities Act Title II (Public Services: State and Local Government)



Fire Station 7

**3901 Palmer Park Blvd
Colorado Springs, CO
80909**

Accessibility Evaluation

***Inspection Date: 03/20/2017
Inspector: Michael Killebrew,
Anna Kangas***

Prepared By



OLYMPIC CITY USA

(719) 385 - 5175

<https://coloradosprings.gov/>

TO: CSFD

FROM: Robert L. Hernandez

DATE: May 31, 2019

SUBJECT: Fire Station 7 Self-Evaluation

1. On March 20, 2017, Mr. Michael Killebrew, Title II ADA/Section 504 Coordinator performed a Title II - Americans with Disabilities Act (ADA), Self-Evaluation. This evaluation was in accordance with 1991/2010 ADA Standards for Accessible Design. The facility was re-evaluated on April 29, 2019 with the assistance of Anna Kangas, Architect/Title II ADA/Section 504 Coordinator.

2. Priorities for accessibility are assigned in accordance with the ADA Checklist for Existing Facilities. The checklist follows the four priorities listed in the Department of Justice ADA Title III regulations. These priorities are equally applicable to state and local government facilities:

Priority 1 - Accessible approach and entrance

Priority 2 - Access to goods and services

Priority 3 - Access to public toilet rooms

Priority 4 - Access to other items such as water fountains and public telephones

3. Corrective actions to findings were/will be submitted for corrective action through the city maintenance work request system and Transition Plan. Note: Recommended corrections in some cases where needed will be modified when appropriate. Corrections are developed in consideration of the "Program Access" provisions of Title II, where applicable.

4. A copy of this report will be maintained in the Office of Accessibility.

City of Colorado Springs Title II ADA/Section 504 – Manager
30 S. Nevada Ave, Suite 301
Colorado Springs, CO 80903
robert.hernandez@coloradosprings.gov

Atch 1. City of Colorado Springs, ADA Self-Evaluation Report

Sincerely,



Robert Hernandez

Exterior - Parking

Lat: 38.8541340000, Long: -104.7570795000

Finding: 1

There are no accessible parking stalls and patients needing blood pressure checks park at the adjacent 7-Eleven. There is limited safe parking in this commercial area.

Each lot where parking is provided for the public as clients, guests or employees, shall provide accessible parking and shall be located on the shortest accessible route of travel from adjacent parking to an accessible entrance.

2010 ADAS Section 208.2

Parking spaces complying with 502 shall be provided in accordance with Table 208.2 except as required by 208.2.1, 208.2.2, and 208.2.3. Where more than one parking facility is provided on a site, the number of accessible spaces provided on the site shall be calculated according to the number of spaces required for each parking facility.

Citation:

2010 ADAS Section: 208.2

As Built:

There are not any accessible parking spaces at the Fire Station.

Recommendation:

Along the southwest corner of the fire station's rear parking lot, stripe three regular parking spaces and 1 van accessible parking space. We do not need signage and anyone can park in the accessible space.

Barrier Priority:

1 (High): Should be completed immediately. (Includes findings that have little or no cost, were in violation of the codes at the time of construction, or pose an imminent safety threat)

Department:

CSFD

Finding #1 Continued



Total Number of Parking Spaces Provided in Parking Facility	Minimum Number of Required Accessible Parking Spaces
1 to 25	1
26 to 50	2
51 to 75	3
76 to 100	4
101 to 150	5
151 to 200	6
201 to 300	7
301 to 400	8
401 to 500	9
501 to 1000	2 percent of total
1001 and over	20, plus 1 for each 100, or fraction thereof, over 1000

Finding 184757 Main Photo

Finding #1 Additional Finding Photos



Finding 184757 Additional Photo

Exterior - Entrance

Lat: 38.8541340000, Long: -104.7570795000

Finding: 2

The door bell is positioned too high for either a side or front approach.

Where a clear floor or ground space allows a parallel approach to an element and the side reach is unobstructed, the high side reach shall be 48 inches maximum and the low side reach shall be 15 inches minimum above the finish floor or ground.

Where a forward reach is unobstructed, the high forward reach shall be 48 inches maximum and the low forward reach shall be 15 inches minimum above the finish floor or ground.

2010 ADAS Section 308.1
Reach ranges shall comply with 308.

Citation:

2010 ADAS Section: 308.1

As Built:

The door bell is mounted at 55".

Recommendation:

Lower the door bell to 48" maximum.

Barrier Priority:

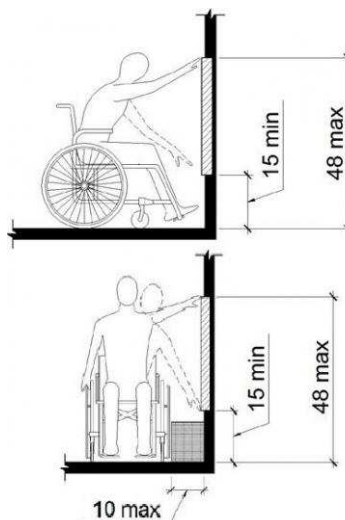
2 (Important): Should be completed as soon as possible. (Includes findings that would remove barriers to the greatest number of people to your goods and services)

Department:

CSFD



Finding 184758 Main Photo



Exterior - Entrance

Lat: 38.8541340000, Long: -104.7570795000

Finding: 3

Door bell should have a sign to make it more conspicuous.

Sign shall be located alongside the door at the latch side. Where a tactile sign is provided at double doors with one active leaf, the sign shall be located on the inactive leaf. Where a tactile sign is provided at double doors with two active leafs, the sign shall be located to the right of the right hand door. Where there is no wall space at the latch side of a single door or at the right side of double doors, signs shall be located on the nearest adjacent wall. Signs containing tactile characters shall be located so that a clear floor space of 18 inches minimum by 18 inches minimum, centered on the tactile characters, is provided beyond the arc of any door swing between the closed position and 45 degree open position.

Wall signs identifying permanent rooms and spaces of a building shall be in a horizontal format and the characters raised 1/32 inch minimum and shall be sans serif uppercase characters a minimum of 5/8 inch and a maximum of 2 inches high. Contracted Grade 2 Braille shall be in a horizontal format and shall be placed a minimum of 3/8 inch and a maximum of 1/2 inch directly below the tactile characters; flush left or centered. Dots shall be 1/10 inch on center in each cell with 2/10-inch space between cells, measured from the second column of dots in the first cell to the first column of dots in the second cell. Dots shall be raised a minimum of 1/40 inch above the background. Braille dots shall be domed or rounded. Signs with raised characters or Braille shall be located 48 inches minimum above the finish floor or ground surface, measured from the baseline of the lowest tactile character and 60 inches maximum above the finish floor or ground surface, measured from the baseline of the highest tactile character.

2010 ADAS Section 216.2

Interior and exterior signs identifying permanent rooms and spaces shall comply with 703.1, 703.2, and 703.5. Where pictograms are provided as designations of permanent interior rooms and spaces, the pictograms shall comply with 703.6 and shall have text descriptors complying with 703.2 and 703.5.

2010 ADAS Section 703.4.2

Where a tactile sign is provided at a door, the sign shall be located alongside the door at the latch side. Where a tactile sign is provided at double doors with one active leaf, the sign shall be located on the inactive leaf. Where a tactile sign is provided at double doors with two active leafs, the sign shall be located to the right of the right hand door. Where there is no wall space at the latch side of a single door or at the right side of double doors, signs shall be located on the nearest adjacent wall. Signs containing tactile characters shall be located so that a clear floor space of 18 inches (455 mm) minimum by 18 inches (455 mm) minimum, centered on the tactile characters, is provided beyond the arc of any door swing between the closed position and 45 degree open position.

Citation:

2010 ADAS Section: 216.2, 703.4.2

As Built:

There is not a wall sign indicating the location of the door bell.

Recommendation:

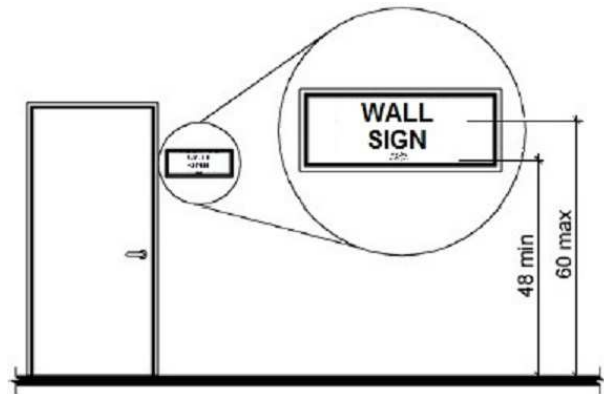
Add information sign indicating door bell.

Barrier Priority:

1 (High): Should be completed immediately. (Includes findings that have little or no cost, were in violation of the codes at the time of construction, or pose an imminent safety threat)

Department:

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Exterior - Entrance

Lat: 38.8541340000, Long: -104.7570795000

Finding: 4

The ash can projects more than 4 inches into the circulation path.

Wall-mounted objects that have leading edges between 27 inches and 80 inches from the floor must not project more than 4 inches into the circulation path. Protruding objects that extend to the floor or within 27 inches of the floor are cane detectable and are therefore not hazardous. Where it is necessary or desirable to have objects protrude from the wall, a manner of cane detection must be provided.

2010 ADAS Section 307.2

Objects with leading edges more than 27 inches (685 mm) and not more than 80 inches (2030 mm) above the finish floor or ground shall protrude 4 inches (100 mm) maximum horizontally into the circulation path.

Citation:

2010 ADAS Section: 307.2

As Built:

The ash can projects more than 4" into the path of travel.

Recommendation:

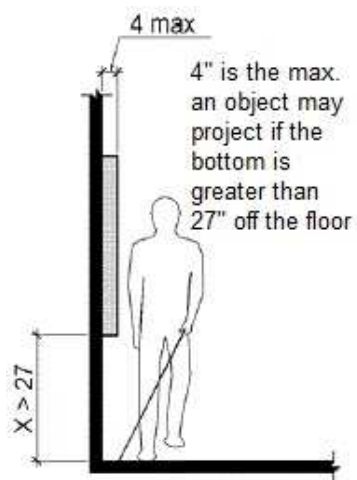
Remove the ash can.

Barrier Priority:

1 (High): Should be completed immediately. (Includes findings that have little or no cost, were in violation of the codes at the time of construction, or pose an imminent safety threat)

Department:

CSFD



Exterior - Entrance

Lat: 38.8541340000, Long: -104.7570795000

Finding: 5

The walkway contains abrupt vertical edges and/or variations over a 1/4 inch.

1/4 inch is the maximum vertical rise. Changes in level between 1/4 inch and 1/2 inch must be beveled at 1:2 or less.

Changes in level greater than 1/2 inch must be by way of a ramp.

2010 ADAS Section 303.3

Changes in level between 1/4 inch (6.4 mm) high minimum and 1/2 inch (13 mm) high maximum shall be beveled with a slope not steeper than 1:2.

2010 ADAS Section 303.2

Changes in level of 1/4 inch (6.4 mm) high maximum shall be permitted to be vertical.

Citation:

2010 ADAS Section: 303.3, 303.2

As Built:

There is 1" vertical displacement in the sidewalk at the entrance.

Recommendation:

cut out and re-pour or add metal bridge plate.

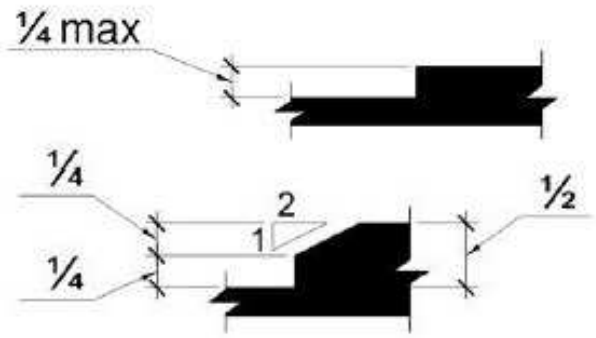
Barrier Priority:

1 (High): Should be completed immediately. (Includes findings that have little or no cost, were in violation of the codes at the time of construction, or pose an imminent safety threat)

Department:

CSFD

Finding #5 Continued



Exterior - Accessible Routes

Lat: 38.8541340000, Long: -104.7570795000

Finding: 6

The walkway contains abrupt vertical edges and/or variations over a 1/4 inch.

1/4 inch is the maximum vertical rise. Changes in level between 1/4 inch and 1/2 inch must be beveled at 1:2 or less.

Changes in level greater than 1/2 inch must be by way of a ramp.

2010 ADAS Section 303.3

Changes in level between 1/4 inch (6.4 mm) high minimum and 1/2 inch (13 mm) high maximum shall be beveled with a slope not steeper than 1:2.

2010 ADAS Section 303.2

Changes in level of 1/4 inch (6.4 mm) high maximum shall be permitted to be vertical.

Citation:

2010 ADAS Section: 303.3, 303.2

As Built:

There is 3/4" vertical displacement where the driveway apron and sidewalk join.

Recommendation:

Remove and replace concrete panels as needed to achieve compliance, or grind edge to less than 1/2".

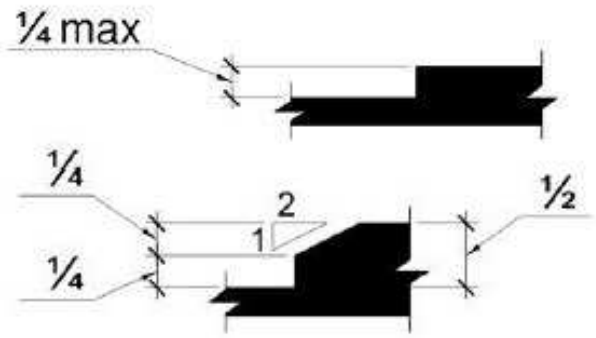
Barrier Priority:

1 (High): Should be completed immediately. (Includes findings that have little or no cost, were in violation of the codes at the time of construction, or pose an imminent safety threat)

Department:

CSFD

Finding #6 Continued



Exterior - Accessible Routes

Lat: 38.8541340000, Long: -104.7570795000

Finding: 7

The accessible route on the site contains running slopes greater than 5% and has not been constructed as a ramp (i.e. handrails, wheel guides, etc.).

When the slope in the direction of travel of any walk exceeds 1 unit vertical in 20 units horizontal (5-percent slope), it must be constructed as ramp. Surface cross slopes must not exceed one unit vertical in 48 units. Horizontal (2-percent slope).

2010 ADAS Section 403.3

The running slope of walking surfaces shall not be steeper than 1:20. The cross slope of walking surfaces shall not be steeper than 1:48.

Citation:

2010 ADAS Section: 403.3

As Built:

The running slope of this section of sidewalk is 6.9%.

Recommendation:

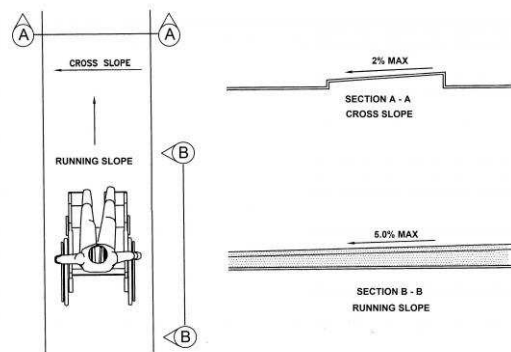
Remove and replace concrete panels as needed to achieve maximum running slope of 5%.

Barrier Priority:

1 (High): Should be completed immediately. (Includes findings that have little or no cost, were in violation of the codes at the time of construction, or pose an imminent safety threat)

Department:

CSFD



Finding #7 Additional Finding Photos



Finding 184763 Additional Photo

Exterior - Accessible Routes

Lat: 38.8541340000, Long: -104.7570795000

Finding: 8

The accessible path of travel contains cross slopes greater than 2%.

Surface cross slopes shall not exceed one unit vertical in 48 units horizontal (2-percent slope). When the slope in the direction of travel of any walk exceeds 1 unit vertical in 20 units horizontal (5-percent slope), it must be constructed as ramp.

2010 ADAS Section 403.3

The running slope of walking surfaces shall not be steeper than 1:20. The cross slope of walking surfaces shall not be steeper than 1:48.

Citation:

2010 ADAS Section: 403.3

As Built:

The cross slope of these six panels exceeds 3%.

Recommendation:

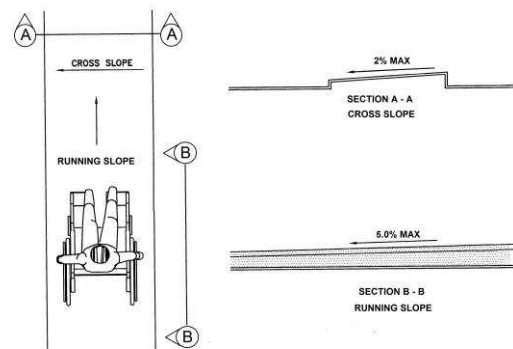
Remove and replace panels as needed to achieve 2% maximum cross slope.

Barrier Priority:

1 (High): Should be completed immediately. (Includes findings that have little or no cost, were in violation of the codes at the time of construction, or pose an imminent safety threat)

Department:

CSFD



Finding #8 Additional Finding Photos



Finding 184768 Additional Photo

Exterior - Accessible Routes

Lat: 38.8541340000, Long: -104.7570795000

Finding: 9

The walkway contains abrupt vertical edges and/or variations over a 1/4 inch.

1/4 inch is the maximum vertical rise. Changes in level between 1/4 inch and 1/2 inch must be beveled at 1:2 or less.

Changes in level greater than 1/2 inch must be by way of a ramp.

2010 ADAS Section 303.3

Changes in level between 1/4 inch (6.4 mm) high minimum and 1/2 inch (13 mm) high maximum shall be beveled with a slope not steeper than 1:2.

2010 ADAS Section 303.2

Changes in level of 1/4 inch (6.4 mm) high maximum shall be permitted to be vertical.

Citation:

2010 ADAS Section: 303.3, 303.2

As Built:

There is considerable spalling in this sidewalk, causing 1/2" or greater vertical displacement.

Recommendation:

This section of sidewalk would be avoided as would the cross slope in this area if we wrap up and use asphalt as the route.

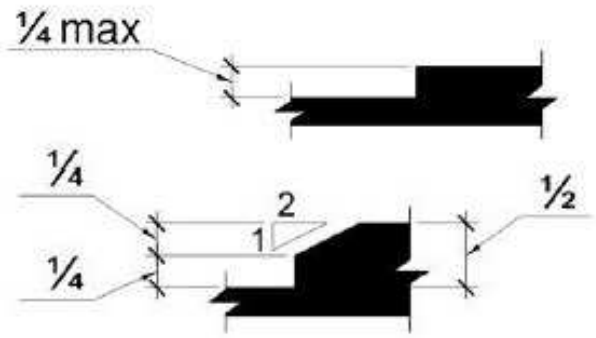
Barrier Priority:

1 (High): Should be completed immediately. (Includes findings that have little or no cost, were in violation of the codes at the time of construction, or pose an imminent safety threat)

Department:

CSFD

Finding #9 Continued



Finding #9 Additional Finding Photos



Finding 184769 Additional Photo

Exterior - Accessible Routes

Lat: 38.8541340000, Long: -104.7570795000

Finding: 10

Detectable warning are missing.

Where a raised pedestrian street crossing, depressed corner, or similar connection between the pedestrian access route at the level of the sidewalk and the level of the pedestrian street crossing that has a grade of 5 percent or less is provided detectable warning the full width and 24 inches in the direction of travel must be installed.

2011 PROWAG Section R208.1

Detectable warning surfaces complying with R305 shall be provided at the following locations on pedestrian access routes and at transit stops:

- 1. Curb ramps and blended transitions at pedestrian street crossings;*
- 2. Pedestrian refuge islands;*
- 3. Pedestrian at-grade rail crossings not located within a street or highway;*
- 4. Boarding platforms at transit stops for buses and rail vehicles where the edges of the boarding platform are not protected by screens or guards; and*
- 5. Boarding and alighting areas at sidewalk or street level transit stops for rail vehicles where the side of the boarding and alighting areas facing the rail vehicles is not protected by screens or guards.*

Advisory R208.1 Where Required. On pedestrian access routes, detectable warning surfaces indicate the boundary between pedestrian and vehicular routes where there is a flush rather than a curbed connection. Detectable warning surfaces should not be provided at crossings of residential driveways since the pedestrian right-of-way continues across residential driveway aprons. However, where commercial driveways are provided with yield or stop control, detectable warning surfaces should be provided at the junction between the pedestrian route and the vehicular route. Where pedestrian at-grade rail crossings are located within a street or highway, detectable warning surfaces at the curb ramps or blended transitions make a second set of detectable warning surfaces at the rail crossing unnecessary.

Detectable warning surfaces are not intended to provide wayfinding for pedestrians who are blind or have low vision. Wayfinding can be made easier by:

- Sidewalks that provide a clear path free of street furniture;*
- Visual contrast between walking and non-walking areas (e.g., planted borders);*
- Route edges that are clear and detectable by cane;*
- Direct pedestrian street crossings and curb ramps that are in-line with direction of travel;*
- Small corner radiuses that permit pedestrian street crossings to be as short and direct as possible;*
- Orthogonal intersections that facilitate navigation using parallel and perpendicular vehicle sound cues;*
- and barriers where pedestrian travel or crossing is not permitted.*

Citation:

2011 PROWAG Section: R208.1

As Built:

There are no detectable warnings on the curb ramp at Palmer Park Blvd.

Recommendation:

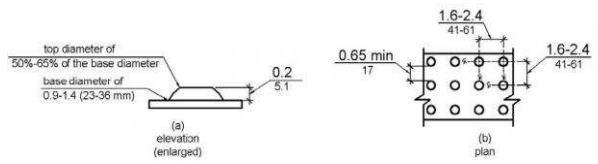
Install detectable warnings.

Barrier Priority:

1 (High): Should be completed immediately. (Includes findings that have little or no cost, were in

violation of the codes at the time of construction, or pose an imminent safety threat)

**Department:
CSFD**



Exterior - Accessible Routes

Lat: 38.8541340000, Long: -104.7570795000

Finding: 11

The curb ramp surface has openings that are greater than 1/2 inch.

There are no detectable warnings on this curb ramp at Academy Blvd.

Openings in floor or ground surfaces shall not allow passage of a sphere more than 1/2 inch diameter. Elongated openings shall be placed so that the long dimension is perpendicular to the dominant direction of travel.

2010 ADAS Section 302.3

Openings in floor or ground surfaces shall not allow passage of a sphere more than 1/2 inch (13 mm) diameter except as allowed in 407.4.3, 409.4.3, 410.4, 810.5.3 and 810.10. Elongated openings shall be placed so that the long dimension is perpendicular to the dominant direction of travel.

Citation:

2010 ADAS Section: 302.3

As Built:

There is a 1" gap at the top of the ramp where it meets the landing.

Recommendation:

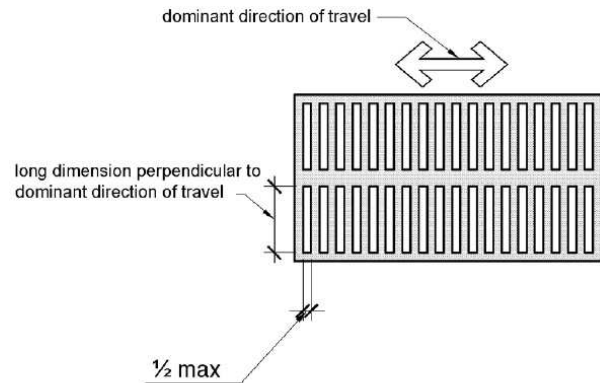
Fill the gap and add detectable warnings.

Barrier Priority:

1 (High): Should be completed immediately. (Includes findings that have little or no cost, were in violation of the codes at the time of construction, or pose an imminent safety threat)

Department:

CSFD



Exterior - Accessible Routes

Lat: 38.8541340000, Long: -104.7570795000

Finding: 12

The building entrance is not located on an accessible route.

When a building, or portion of a building, is required to be accessible, an accessible route of travel shall be provided to all portions of the building, to accessible building entrances and between the building and the public way.

All walks, halls, corridors, aisles, skywalks, tunnels, and other spaces that are part of an accessible route must comply. An accessible route of travel must not pass through kitchens, storage rooms, restrooms, closets or other spaces used for similar purposes. At least one accessible route within the boundary of the site must be provided from public transportation stops, accessible parking and accessible passenger loading zones and public streets or sidewalks to the accessible building entrance they serve.

The accessible route shall, to the maximum extent feasible, coincide with the route for the general public. At least one accessible route shall connect accessible buildings, facilities, elements and spaces that are on the same site. At least one accessible route shall connect accessible building or facility entrances with all accessible spaces and elements.

2010 ADAS Section 206.2.1

At least one accessible route shall be provided within the site from accessible parking spaces and accessible passenger loading zones; public streets and sidewalks; and public transportation stops to the accessible building or facility entrance they serve.

Citation:

2010 ADAS Section: 206.2.1

As Built:

There is not an accessible route from the current and proposed parking to the fire station or in the public right of way

Recommendation:

Put in AC route from parking area behind fire station (FS); explore installing AC route inside FS perimeter along west side of fence. If not in the interior we should look at adding sidewalk on exterior of fence in the PROW.

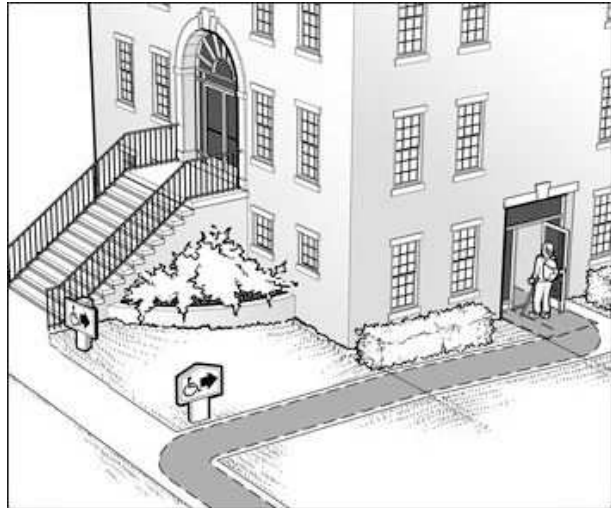
Barrier Priority:

1 (High): Should be completed immediately. (Includes findings that have little or no cost, were in violation of the codes at the time of construction, or pose an imminent safety threat)

Department:

CSFD

Finding #12 Continued



Finding 184767 Main Photo

Finding #12 Additional Finding Photos



Finding 184767 Additional Photo

Exterior - Accessible Routes

Lat: 38.8541340000, Long: -104.7570795000

Finding: 13

The surface of the curb ramp is not firm, stable and/or slip resistant..
The curb ramp also is missing detectable warnings.

Floor and ground surfaces shall be stable, firm, and slip resistant.

2010 ADAS Section 302.1

Floor and ground surfaces shall be stable, firm, and slip resistant and shall comply with 302.

Citation:

2010 ADAS Section: 302.1

As Built:

There is considerable debris build-up at the curb ramp at Academy Blvd and Palmer Park Blvd.

Recommendation:

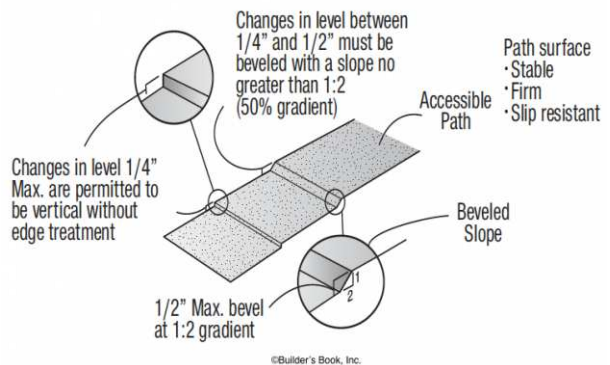
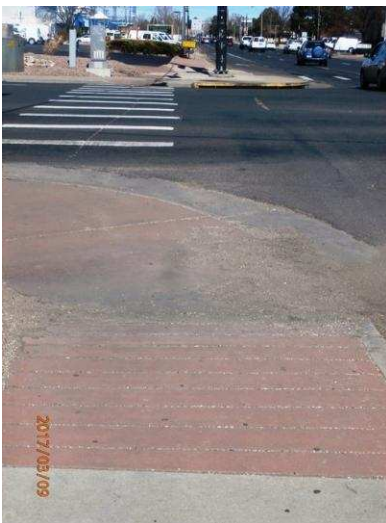
Keep the curb ramp clear of debris. This may require evaluating and re-routing stormwater runoff or moving/completely redoing the ramp. Also detectable warnings need to be added to the ramp.

Barrier Priority:

1 (High): Should be completed immediately. (Includes findings that have little or no cost, were in violation of the codes at the time of construction, or pose an imminent safety threat)

Department:

CSFD



Interior - Restrooms

Lat: 38.8541340000, Long: -104.7570795000

Finding: 14

There are multiple issues with the restrooms but they are not intended for public use.

The owner or owners representative indicated that no public restrooms were available, however there was a non compliant restroom labeled as accessible and available to the public. If the restroom is desired to be an employee only restroom, it is recommended that the door be locked, the signs be removed and replaced with a new sign that that sates "employees only" or "private area". Any future improvements may be required under Title I of the ADA for work place reasonable accommodations.

Bathing and toilet facilities that serve buildings, facilities or portions of buildings or facilities that are required by these standards to be accessible to persons with disabilities, shall be on an accessible route.

2010 ADAS Section 213.1

Where toilet facilities and bathing facilities are provided, they shall comply with 213. Where toilet facilities and bathing facilities are provided in facilities permitted by 206.2.3 Exceptions 1 and 2 not to connect stories by an accessible route, toilet facilities and bathing facilities shall be provided on a story connected by an accessible route to an accessible entrance.

Citation:

2010 ADAS Section: 213.1

As Built:

The restrooms are not accessible.

Recommendation:

Add signage indicating the restrooms are not for public use.

Barrier Priority:

3 (Moderate): Should be completed as soon as possible, but there may be other items that will provide greater access to persons with disabilities. (Includes findings that have a high financial impact on the entity in relationship to the degree of access provided)

Department:

CSFD

Finding #14 Continued



Finding #14 Additional Finding Photos



Finding 184772 Additional Photo



Finding 184772 Additional Photo



Finding 184772 Additional Photo



Finding 184772 Additional Photo