

Site Accessibility Evaluation

Americans with Disabilities Act Title II (Public Services: State and Local Government)



**Pikes Peak Community
College Transfer Station
PU18-0083**

**5675 S Academy Blvd
Colorado Springs, CO
80906**

Transit Stops

***Inspection Date: 06/13/2016
Inspector: Michael Killebrew,
Anna Kangas***

Prepared By



(719) 385 - 5175

<https://coloradosprings.gov/>

TO: Mountain Metropolitan Transit

FROM: Robert L. Hernandez

DATE: January 15, 2020

SUBJECT: Pikes Peak Community College Transfer Station Self-Evaluation

1. On June 13, 2016, Mr. Michael Killebrew, Title II ADA/Section 504 Coordinator performed a Title II - Americans with Disabilities Act (ADA), Self-Evaluation. This evaluation was in accordance with 1991/2010 ADA Standards for Accessible Design. The facility was re-evaluated on January 7, 2020 with the assistance of Anna Kangas, Architect/Title II ADA/Section 504 Coordinator.

2. Priorities for accessibility are assigned in accordance with the ADA Checklist for Existing Facilities. The checklist follows the four priorities listed in the Department of Justice ADA Title III regulations. These priorities are equally applicable to state and local government facilities:

Priority 1 - Accessible approach and entrance

Priority 2 - Access to goods and services

Priority 3 - Access to public toilet rooms

Priority 4 - Access to other items such as water fountains and public telephones

3. Corrective actions to findings were/will be submitted for corrective action through the city maintenance work request system and Transition Plan. Note: Recommended corrections in some cases where needed will be modified when appropriate. Corrections are developed in consideration of the "Program Access" provisions of Title II, where applicable.

4. A copy of this report will be maintained in the Office of Accessibility.

City of Colorado Springs Title II ADA/Section 504 – Manager
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Atch 1. City of Colorado Springs, ADA Self-Evaluation Report

Sincerely,



Robert Hernandez

Exterior - Accessible Routes

Lat: 38.7646232000, Long: -104.7862045000

Finding: 1

There are multiple issues with this curb ramp including:

The top of the curb ramp has 1" vertical displacement with a 1-1/4" wide gap.

The bottom of the curb ramp has 3/4" vertical displacement and 3/4" wide gap, and is deteriorating.

*2010 ADAS Section 406
Curb Ramps*

Citation:

2010 ADAS Section: 406

As Built:

There are multiple issues with this curb ramp from the parking area to the transfer station.

Recommendation:

Remove and replace the curb ramp.

Barrier Priority:

1 (High): Should be completed immediately. (Includes findings that have little or no cost, were in violation of the codes at the time of construction, or pose an imminent safety threat)



Image of noncompliant curb ramp

Finding #1 Additional Finding Photos



Additional image of noncompliant curb ramp

Exterior - Accessible Routes

Lat: 38.7646232000, Long: -104.7862045000

Finding: 2

The concrete expansion joint is spaced greater than 1/2 inch.

Openings in floor or ground surfaces shall not allow passage of a sphere more than 1/2 inch diameter. Elongated openings shall be placed so that the long dimension is perpendicular to the dominant direction of travel.

2010 ADAS Section 302.3

Openings in floor or ground surfaces shall not allow passage of a sphere more than 1/2 inch (13 mm) diameter except as allowed in 407.4.3, 409.4.3, 410.4, 810.5.3 and 810.10. Elongated openings shall be placed so that the long dimension is perpendicular to the dominant direction of travel.

Citation:

2010 ADAS Section: 302.3

As Built:

There is a 1" wide gap for the length of 7 panels.

Recommendation:

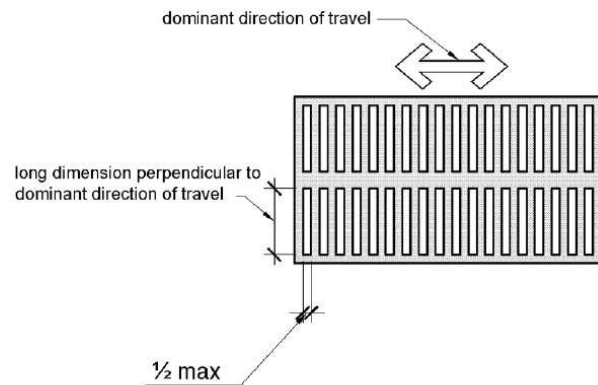
Clean out and fill the gap.

Barrier Priority:

1 (High): Should be completed immediately. (Includes findings that have little or no cost, were in violation of the codes at the time of construction, or pose an imminent safety threat)



Image of gap in sidewalk



Finding #2 Additional Finding Photos



Additional image of gaps in sidewalk



Additional image of gaps in sidewalk

Exterior - Accessible Routes

Lat: 38.7646232000, Long: -104.7862045000

Finding: 3

The outdoor seating area is not on an accessible route.

At least 5% of the seating in each functional area must be accessible.

2010 ADAS Section 226.1

Where dining surfaces are provided for the consumption of food or drink, at least 5 percent of the seating spaces and standing spaces at the dining surfaces shall comply with 902. In addition, where work surfaces are provided for use by other than employees, at least 5 percent shall comply with 902.

Citation:

2010 ADAS Section: 226.1

As Built:

In front of the bus shelter, there is 1" vertical displacement.

Recommendation:

Grind to less than 1/2".

Barrier Priority:

1 (High): Should be completed immediately. (Includes findings that have little or no cost, were in violation of the codes at the time of construction, or pose an imminent safety threat)



Image of vertical displacement at bus stop

Finding #3 Additional Finding Photos



Additional image of displacement at bus stop

Exterior - Accessible Routes

Lat: 38.7646232000, Long: -104.7862045000

Finding: 4

The accessible route contains running slopes greater than 5 percent and cross slopes greater than 2 percent across the width of the walk and has not been constructed as a ramp (i.e. handrails, wheel guides, etc.).

When the slope in the direction of travel of any walk exceeds 1 unit vertical in 20 units horizontal (5-percent slope), it must be constructed as ramp. Surface cross slopes must not exceed one unit vertical in 48 units. Horizontal (2-percent slope).

2010 ADAS Section 403.3

The running slope of walking surfaces shall not be steeper than 1:20. The cross slope of walking surfaces shall not be steeper than 1:48.

Citation:

2010 ADAS Section: 403.3

As Built:

The running and cross slopes of 2 panels are up to 4%.

Recommendation:

Remove and replace the panels to provide maximum 2% slopes.

Barrier Priority:

1 (High): Should be completed immediately. (Includes findings that have little or no cost, were in violation of the codes at the time of construction, or pose an imminent safety threat)

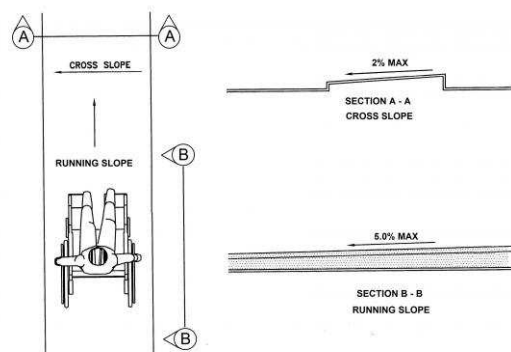


Image of sidewalk with excessive running and cross slopes

Exterior - Accessible Routes

Lat: 38.7646232000, Long: -104.7862045000

Finding: 5

The surface of the pedestrian access route has openings greater than 1/2 inch.

2011 PROWAG Section R302.7.3

Horizontal openings in gratings and joints shall not permit passage of a sphere more than 13 mm (0.5 in) in diameter. Elongated openings in gratings shall be placed so that the long dimension is perpendicular to the dominant direction of travel.

Citation:

2011 PROWAG Section: R302.7.3

As Built:

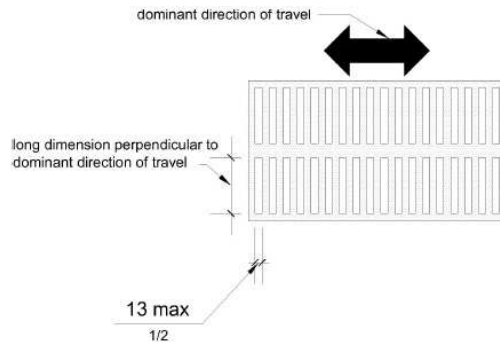
Near the ticket kiosk, there are up to 1-1/2" wide gaps and up to 1" vertical displacement.

Recommendation:

Grind to less than 1/2" bevel. Clean out and fill gap.

Barrier Priority:

1 (High): Should be completed immediately. (Includes findings that have little or no cost, were in violation of the codes at the time of construction, or pose an imminent safety threat)



Exterior - Accessible Routes

Lat: 38.7646232000, Long: -104.7862045000

Finding: 6

The trash can is positioned too high for either a side or front approach.

Where a clear floor or ground space allows a parallel approach to an element and the side reach is unobstructed, the high side reach shall be 48 inches maximum and the low side reach shall be 15 inches minimum above the finish floor or ground.

Where a forward reach is unobstructed, the high forward reach shall be 48 inches maximum and the low forward reach shall be 15 inches minimum above the finish floor or ground.

*2010 ADAS Section 308.1
Reach ranges shall comply with 308.*

Citation:

2010 ADAS Section: 308.1

As Built:

The trash can on the right of the last shelter is 50-1/2" to the operable portion.

Recommendation:

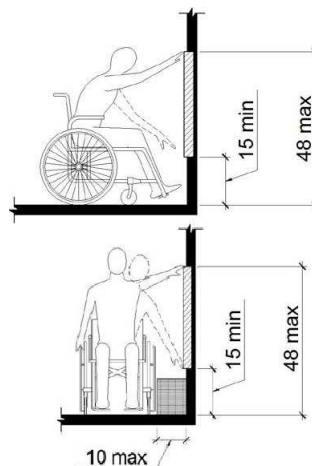
The trash can is not required to meet reach ranges. However, when this trash can is replaced, replace with one that is maximum 48" to the operable part.

Barrier Priority:

4 (Low): Should be completed as soon as possible due to being a technical violation, but may not result in providing greater access to persons with disabilities. (Includes findings that are technically violations but provide a moderate to low increase in accessibility compared to the financial impact on the entity)



Image of bus shelter trash can



Exterior - Accessible Routes

Lat: 38.7646232000, Long: -104.7862045000

Finding: 7

There are 6 panels with slopes between 2% and 3%.

There is a gap up to 1-3/4" wide, up to 1-1/2" deep, with up to 3/4" vertical displacement.

2011 PROWAG Section R302.7.2

Vertical surface discontinuities shall be 13 mm (0.5 in) maximum. Vertical surface discontinuities between 6.4 mm (0.25 in) and 13 mm (0.5 in) shall be beveled with a slope not steeper than 50 percent. The bevel shall be applied across the entire vertical surface discontinuity. Advisory R302.7.2 Vertical Surface Discontinuities. The allowance for vertical surface discontinuities is for occasional expansion joints and objects such as utility covers, vault frames, and gratings that cannot be located in another portion of the sidewalk outside the pedestrian access route. However, objects such as utility covers, vault frames, and gratings should not be located on curb ramp runs, blended transitions, turning spaces, or gutter areas within the pedestrian access route. This may not always be possible in alterations, but should be avoided wherever possible. Vertical surface discontinuities between unit pavers should be minimized.

Citation:

2011 PROWAG Section: R302.7.2

As Built:

There are several issues with the sidewalk near the last shelter.

Recommendation:

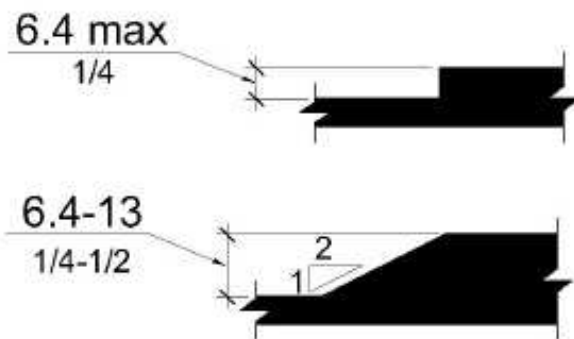
Remove and replace the panels.

Barrier Priority:

1 (High): Should be completed immediately. (Includes findings that have little or no cost, were in violation of the codes at the time of construction, or pose an imminent safety threat)



Image of sidewalk with excessive slopes



Exterior - Accessible Routes

Lat: 38.7646232000, Long: -104.7862045000

Finding: 8

There are gaps up to 1-1/2" wide, and up to 1/2" vertical displacement.

2011 PROWAG Section R302.7.2

Vertical surface discontinuities shall be 13 mm (0.5 in) maximum. Vertical surface discontinuities between 6.4 mm (0.25 in) and 13 mm (0.5 in) shall be beveled with a slope not steeper than 50 percent. The bevel shall be applied across the entire vertical surface discontinuity. Advisory R302.7.2 Vertical Surface Discontinuities. The allowance for vertical surface discontinuities is for occasional expansion joints and objects such as utility covers, vault frames, and gratings that cannot be located in another portion of the sidewalk outside the pedestrian access route. However, objects such as utility covers, vault frames, and gratings should not be located on curb ramp runs, blended transitions, turning spaces, or gutter areas within the pedestrian access route. This may not always be possible in alterations, but should be avoided wherever possible. Vertical surface discontinuities between unit pavers should be minimized.

Citation:

2011 PROWAG Section: R302.7.2

As Built:

Near bay #35, there are multiple issues with the sidewalk.

Recommendation:

Grind to less than 1/2" vertical displacement. Clean out and fill gap.

Barrier Priority:

1 (High): Should be completed immediately. (Includes findings that have little or no cost, were in violation of the codes at the time of construction, or pose an imminent safety threat)

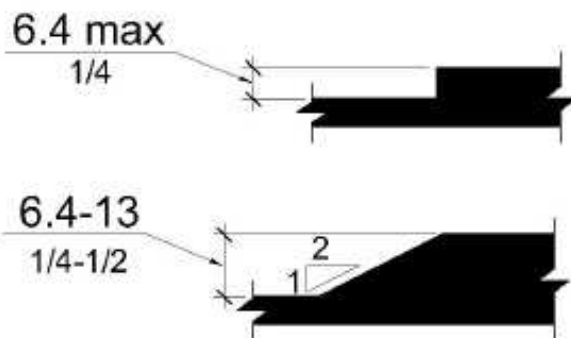


Image of gaps and vertical displacement in sidewalk

Finding #8 Additional Finding Photos



Additional image of gaps and vertical displacement

Exterior - Accessible Routes

Lat: 38.7646232000, Long: -104.7862045000

Finding: 9

There is a 1" gap and up to 3/4" vertical displacement.

2011 PROWAG Section R302.7.2

Vertical surface discontinuities shall be 13 mm (0.5 in) maximum. Vertical surface discontinuities between 6.4 mm (0.25 in) and 13 mm (0.5 in) shall be beveled with a slope not steeper than 50 percent. The bevel shall be applied across the entire vertical surface discontinuity. Advisory R302.7.2 Vertical Surface Discontinuities. The allowance for vertical surface discontinuities is for occasional expansion joints and objects such as utility covers, vault frames, and gratings that cannot be located in another portion of the sidewalk outside the pedestrian access route. However, objects such as utility covers, vault frames, and gratings should not be located on curb ramp runs, blended transitions, turning spaces, or gutter areas within the pedestrian access route. This may not always be possible in alterations, but should be avoided wherever possible. Vertical surface discontinuities between unit pavers should be minimized.

Citation:

2011 PROWAG Section: R302.7.2

As Built:

There are multiple issues with the sidewalk near bay #27.

Recommendation:

Grind to less than 1/2". Clean out and fill gaps.

Barrier Priority:

1 (High): Should be completed immediately. (Includes findings that have little or no cost, were in violation of the codes at the time of construction, or pose an imminent safety threat)

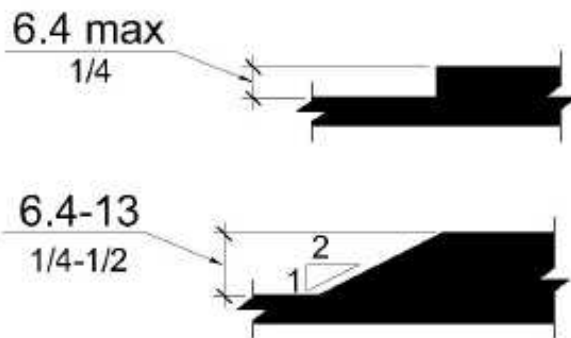


Image of gaps and vertical displacement in sidewalk.

Finding #9 Additional Finding Photos



Additional image of gaps and vertical displacement

Exterior - Accessible Routes

Lat: 38.7646232000, Long: -104.7862045000

Finding: 10

The bus schedules are positioned too high for either a side or front approach.

Where a clear floor or ground space allows a parallel approach to an element and the side reach is unobstructed, the high side reach shall be 48 inches maximum and the low side reach shall be 15 inches minimum above the finish floor or ground.

Where a forward reach is unobstructed, the high forward reach shall be 48 inches maximum and the low forward reach shall be 15 inches minimum above the finish floor or ground.

*2010 ADAS Section 308.1
Reach ranges shall comply with 308.*

Citation:

2010 ADAS Section: 308.1

As Built:

The reach range to the bus schedules is 52".

Recommendation:

Lower the bus schedule box to 48" maximum.

Barrier Priority:

2 (Important): Should be completed as soon as possible. (Includes findings that would remove barriers to the greatest number of people to your goods and services)

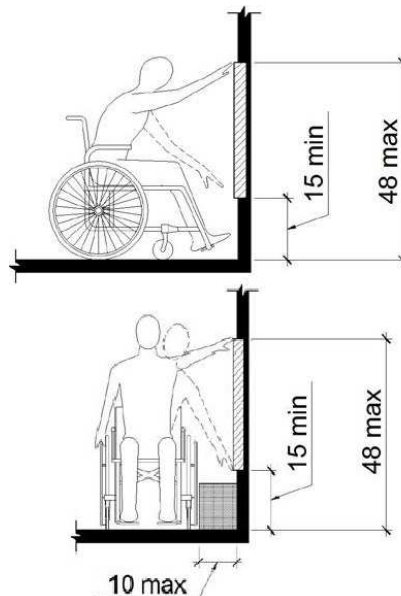


Image of bus stop schedules mounted too high