

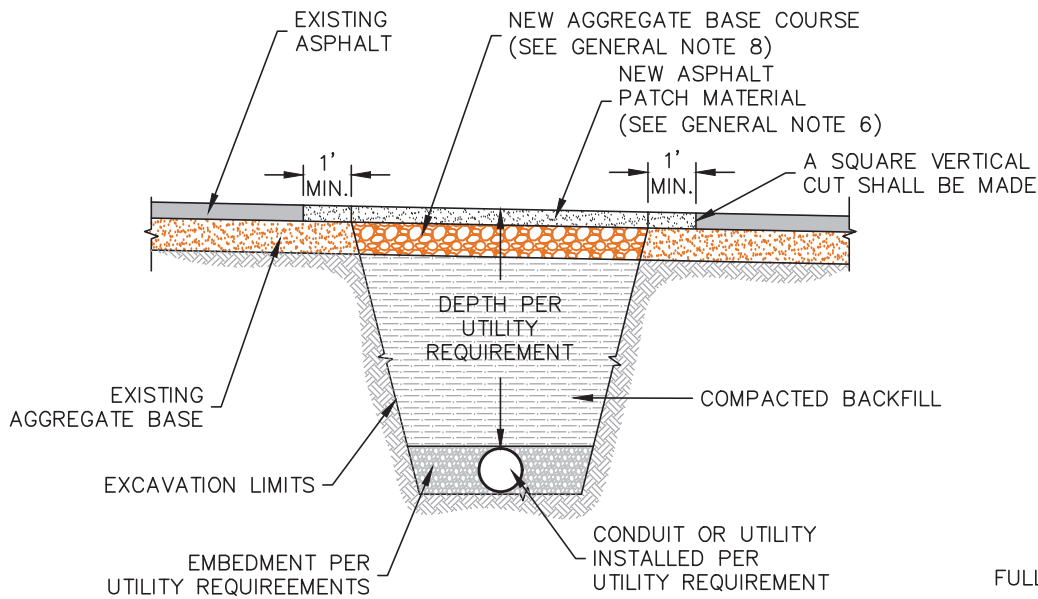
**General Notes:**

1. ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE CURRENT CITY OF COLORADO SPRINGS ENGINEERING DIVISION (THE CITY) STANDARD SPECIFICATIONS.
2. THE CONTRACTOR SHALL OBTAIN ALL REQUIRED PERMITS. FOR CITY PERMITS CONTRACTORS SHALL USE THE ACCELA ON-LINE PERMIT SYSTEM. ONCE CITY PERMITS ARE APPROVED AND PAID, THEN APPROPRIATE SCHEDULING AND NOTIFICATIONS SHALL BE IN ACCELA.
3. PRIOR TO CUTTING INTO A ROADWAY, A MEETING IS REQUIRED WITH CITY INSPECTOR.
4. EXISTING PAVEMENT MAY BE INITIALLY ROUGH CUT. A SQUARE, VERTICAL CUT SHALL BE MADE IN THE EXISTING ASPHALT PAVEMENT PRIOR TO PAVEMENT PLACEMENT.
5. BACKFILL SHALL BE COMPACTED WITH SECTION 206 OF CITY STANDARD SPECIFICATIONS.
6. CLSM (FLOW-FILL) IS REQUIRED AS BACKFILL FOR TRENCHES LESS THAN 1-FOOT IN WIDTH. CLSM SHALL NOT EXTEND INTO PAVEMENT SECTION.
7. CLSM SHALL BE USED WHERE PAVEMENT SECTION IS LESS THAN 5-YEARS OLD.
8. HOT MIX ASPHALT (ASPHALT PAVING MATERIAL) SHALL MEET THE REQUIREMENTS OF THE PIKES PEAK ASPHALT SPECIFICATION AND BE APPROVED BY CITY ENGINEERING.
9. NEW ASPHALT SHALL NOT BE PLACED AGAINST FRESHLY POURED CONCRETE. CONCRETE SHALL BE 5-DAYS OLD OR HAVE REACHED A COMPRESSIVE STRENGTH OF 3,200 PSI AS DEMONSTRATED BY FIELD CURE CYLINDERS.
10. A TACK COAT SHALL BE APPLIED TO ALL VERTICAL EDGES INCLUDING CONCRETE EDGES. THE TACK COAT SHALL HAVE 100% COVERAGE AND BE APPLIED BETWEEN LIFTS.
11. A MINIMUM PAVEMENT SECTION OF:
  - A) RESIDENTIAL/COLLECTOR...SHALL MATCH EXISTING OR A MINIMUM 6-INCHES OF CLASS 6 AGGREGATE BASE COURSE WITH 6-INCHES OF HOT-MIX ASPHALT (ASPHALT PAVING MATERIAL);
  - B) ARTERIAL...12-INCHES OF CLASS 6 AGGREGATE BASE COURSE WITH 8-INCHES OF HOT-MIX ASPHALT (ASPHALT PAVING MATERIAL).
12. THE NEW PAVEMENT SECTION SHALL MATCH EXISTING PAVEMENT REPORT. FOR ROADWAYS WHERE THE SUBGRADE IS CHEMICALLY TREATED (CTS), CTS OR FLOW-FILL SHALL BE EQUIVALENT TO THE REQUIREMENT IN THE PAVEMENT DESIGN REPORT.
13. ALTERNATE REPAIR SECTIONS MAY BE APPROVED PROVIDED THEY ARE COMPLETED BY THE PAVEMENT DESIGN GEOTECHNICAL ENGINEER, AND APPROVED BY CITY ENGINEERING.
14. NEW HOT-MIXED ASPHALT (ASPHALT PAVING MATERIAL) SHALL BE FLUSH TO EXISTING ASPHALT AND CONCRETE EDGES AND SHALL NOT HAVE HUMPS OR VALLEYS.
15. WHERE CONCRETE PAVEMENT EXISTS BELOW THE ASPHALT, NEW CONCRETE SHALL BE PLACED TO MATCH THE EXISTING CONCRETE THICKNESS UNLESS OTHERWISE APPROVED BY CITY ENGINEERING.
16. IF A PLATE TAMPER IS USED FOR COMPACTION OF ASPHALT, THE MAXIMUM LOOSE LIFT THICKNESS SHALL BE 2-INCHES.
17. THESE DETAILS ARE FOR PAVEMENT CUTS LESS THAN 200-SF. CUTS GREATER THAN 200-SF SHALL BE IN CONFORMANCE WITH AN ENGINEERED DESIGN.
18. ANY DISTURBED PAVEMENT MARKINGS SHALL BE RESTORED TO CITY STANDARDS.

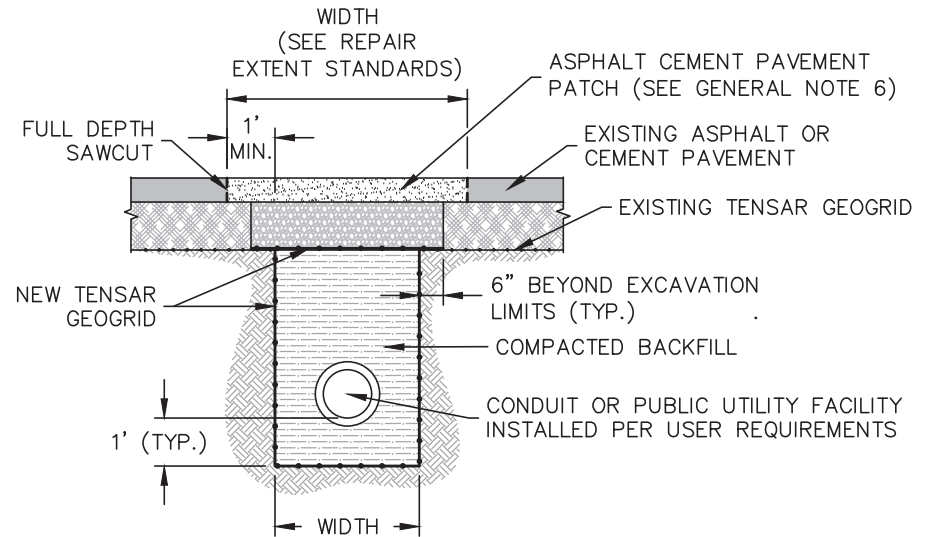


UTILITY TRENCH REPAIR  
GENERAL NOTES

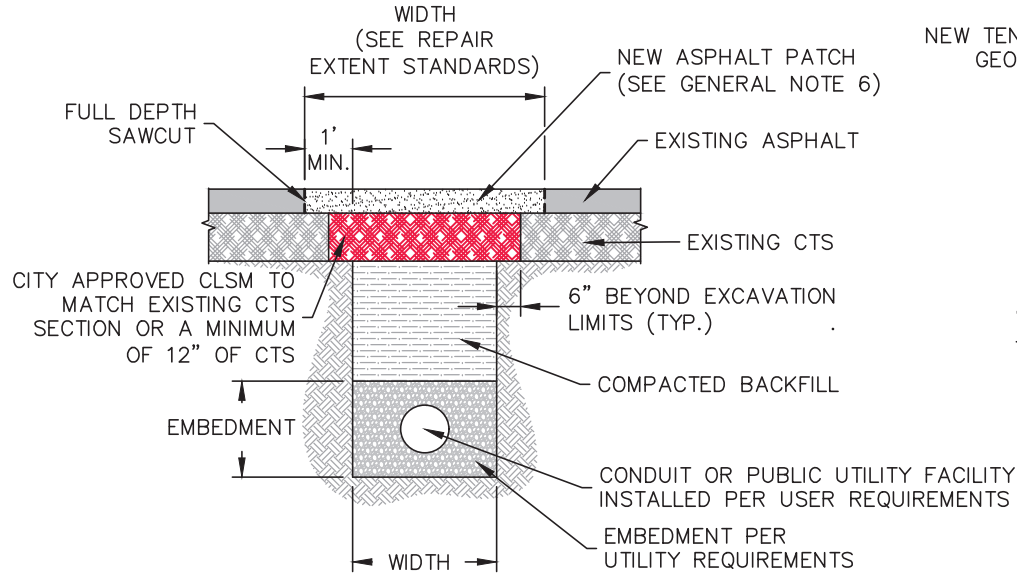
APPROVED: <i>Jayle Sturdivant</i> CITY ENGINEER		
ISSUED: 11/2/20	REVISED:	DRAWING NO. 4A




**UTILITY TRENCH  
PAVEMENT REPAIR DETAIL**

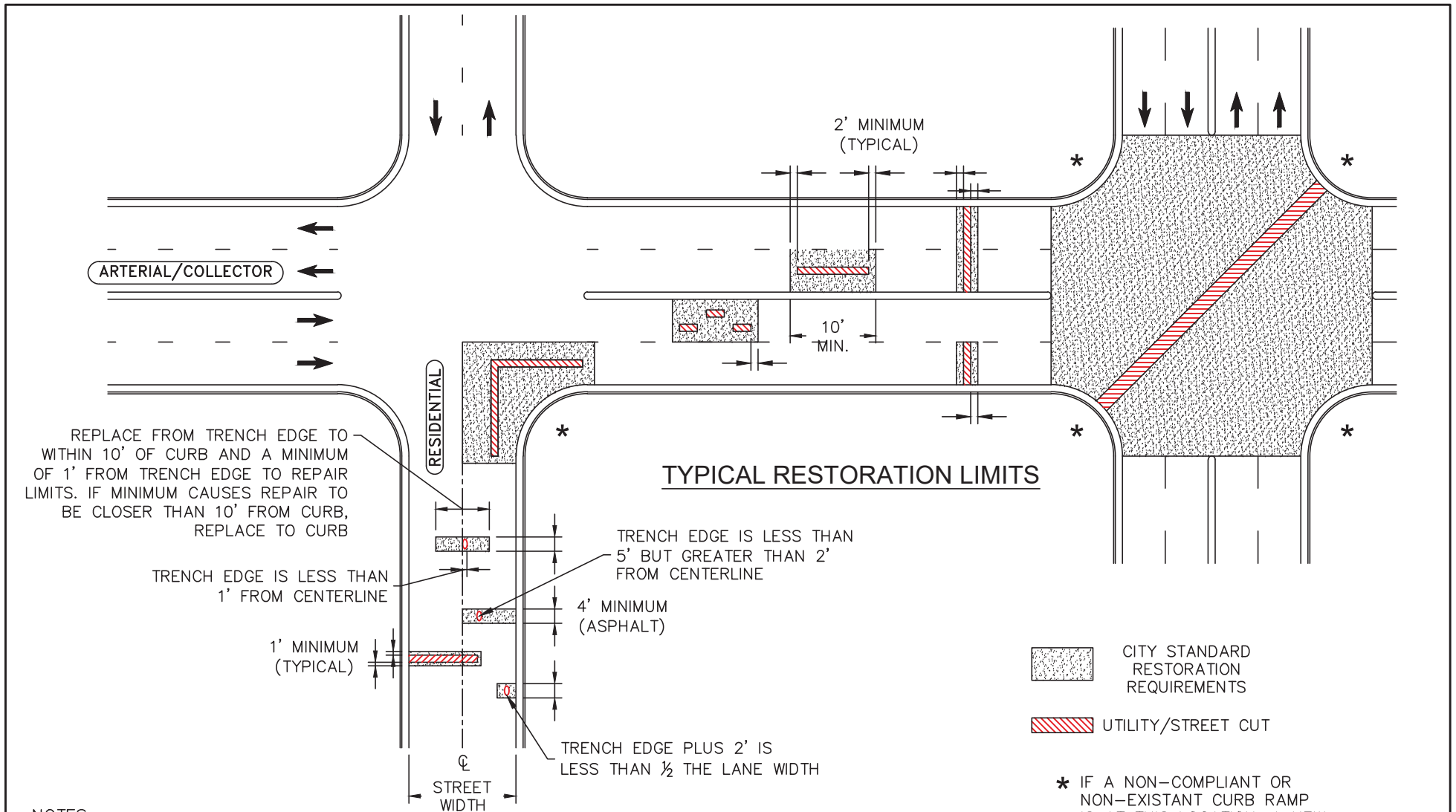


**TYPICAL UTILITY TRENCH REPAIR DETAIL  
(GEO-GRID SECTION)**



**UTILITY TRENCH PAVEMENT REPAIR DETAIL  
CHEMICALLY TREATED SUBGRADE**


	<b>UTILITY TRENCH REPAIR TYPICAL SECTIONS</b>	
	APPROVED: <i>Gayle Sturdivant</i> CITY ENGINEER	
	ISSUED: 11/2/20	REVISED:
		DRAWING NO. 4B



**NOTES**

1. REMOVE AND REPLACE A MINIMUM OF 4-FEET LONGITUDINAL OR 2-FEET FROM THE EDGE OF THE TRENCH, WHICHEVER IS GREATER.
2. IF WITHIN 5-FEET OF AN EXISTING JOINT THEN REMOVE TO THE EXISTING JOINT.
3. MULTIPLE LOCATIONS ARE TO BE A MINIMUM OF 10-FEET APART FROM EDGE OF REPAIR TO EDGE OF REPAIR; IF REPAIRS ARE LESS THAN 10-FEET APART, A CONTINUOUS SECTION MUST BE REPLACED.
4. CURB & GUTTER MAY REMAIN, PROVIDED THE CURB AND GUTTER ARE NOT DAMAGED BY THE CONSTRUCTION ACTIVITY.
5. EXACT RESTORATION LIMITS/LOCATIONS SHALL BE APPROVED BY THE CITY REPRESENTATIVE PRIOR TO REPAIRS.
6. LONGITUDINAL REPAIRS GREATER THAN 40-FEET IN LENGTH SHALL BE MILLED AND PAVED.

\* IF A NON-COMPLIANT OR NON-EXISTANT CURB RAMP IS AT THIS LOCATION, A NEW RAMP SHALL BE INSTALLED AS PART OF THE PROJECT.

	<b>UTILITY TRENCH REPAIR RESTORATION LIMITS</b>	
	APPROVED: <i>Jayle Sturdivant</i> <small>CITY ENGINEER</small>	
	ISSUED: 11/2/20	REVISED: