

# Site Accessibility Evaluation

## Americans with Disabilities Act Title II (Public Services: State and Local Government)



**Woodmen Park & Ride  
PU18-0089**

**6995 Mark Dabling Blvd  
Northeast Colorado  
Springs, CO 80919**

**Transit Stops**

*Inspection Date: 06/14/2016  
Inspector: Michael Killebrew,  
Anna Kangas*

**Prepared By**



**(719) 385 - 5175**

**<https://coloradosprings.gov/>**

TO: Mountain Metropolitan Transit

FROM: Robert L. Hernandez

DATE: January 15, 2020

SUBJECT: Woodmen Park & Ride Self-Evaluation

1. On June 14, 2016, Mr. Michael Killebrew, Title II ADA/Section 504 Coordinator performed a Title II - Americans with Disabilities Act (ADA), Self-Evaluation. This evaluation was in accordance with 1991/2010 ADA Standards for Accessible Design. The facility was re-evaluated on December 26, 2019 with the assistance of Anna Kangas, Architect/Title II ADA/Section 504 Coordinator.

2. Priorities for accessibility are assigned in accordance with the ADA Checklist for Existing Facilities. The checklist follows the four priorities listed in the Department of Justice ADA Title III regulations. These priorities are equally applicable to state and local government facilities:

Priority 1 - Accessible approach and entrance

Priority 2 - Access to goods and services

Priority 3 - Access to public toilet rooms

Priority 4 - Access to other items such as water fountains and public telephones

3. Corrective actions to findings were/will be submitted for corrective action through the city maintenance work request system and Transition Plan. Note: Recommended corrections in some cases where needed will be modified when appropriate. Corrections are developed in consideration of the "Program Access" provisions of Title II, where applicable.

4. A copy of this report will be maintained in the Office of Accessibility.

City of Colorado Springs Title II ADA/Section 504 – Manager  
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Atch 1. City of Colorado Springs, ADA Self-Evaluation Report

Sincerely,



Robert Hernandez

## Parking

Lat: 38.9312782000, Long: -104.8158318000

### Finding: 1

The access aisles are not a minimum 5 feet wide. One of the access aisles is 4'-6" wide, and one is 4'-3" wide. However, there is an extra access aisle at the end of the accessible parking.

A accessible parking stall's access aisle must be a minimum of 5 feet wide measured from centerline to centerline.

Where the access aisle is not adjacent to another parking space or access aisle, measurements shall be permitted to include the full width of the line defining the access aisle.

2010 ADAS Section 502.3.1

Access aisles serving car and van parking spaces shall be 60 inches (1525 mm) wide minimum.

### Citation:

2010 ADAS Section: 502.3.1

### As Built:

There are two access aisles that are too narrow.

### Recommendation:

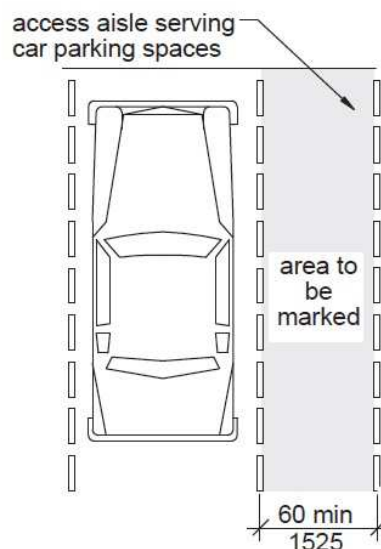
Restripe the accessible parking and access aisles, reducing the width of the extra access aisle, and adding width to the two narrow access aisles.

### Barrier Priority:

1 (High): Should be completed immediately. (Includes findings that have little or no cost, were in violation of the codes at the time of construction, or pose an imminent safety threat)



Image of narrow access aisles



## Parking

**Lat: 38.9312782000, Long: -104.8158318000**

### Finding: 2

The accessible parking signage is mounted higher than 7'-0" to the bottom of the lowest sign.

The signage on the left parking space is mounted at 8'-2" to the bottom of the sign.

The signage on the 2nd from left parking space is mounted at 7'-8" to the bottom of the sign.

The signage on the 3rd from left parking space is mounted at 7'-6" to the bottom of the sign.

The signage on the far right parking space is acceptable, at 6'-7".

*2010 ADAS Section 502.6*

*Parking space identification signs shall include the International Symbol of Accessibility complying with 703.7.2.1. Signs identifying van parking spaces shall contain the designation "van accessible." Signs shall be 60 inches (1525 mm) minimum above the finish floor or ground surface measured to the bottom of the sign.*

#### Citation:

2010 ADAS Section: 502.6

#### As Built:

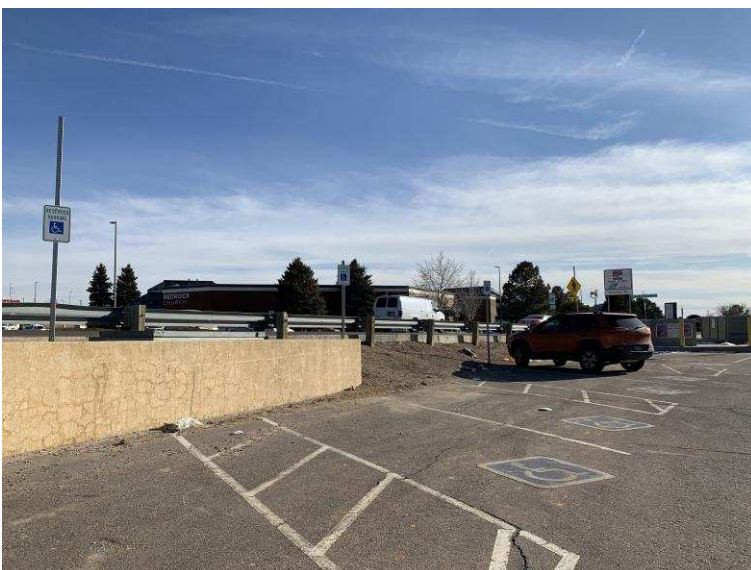
The accessible parking signage is mounted too high.

#### Recommendation:

Lower the signage to between 5'-0" and 7'-0" to the bottom of the lowest sign.

#### Barrier Priority:

1 (High): Should be completed immediately. (Includes findings that have little or no cost, were in violation of the codes at the time of construction, or pose an imminent safety threat)



## **Parking**

**Lat: 38.9312782000, Long: -104.8158318000**

### **Finding: 3**

The surfaces of the accessible parking spaces and access aisles are not firm, stable, and slip resistant.

**Floor and ground surfaces shall be stable, firm, and slip resistant**

**Parking spaces and access aisles serving them shall comply with 302. Access aisles shall be at the same level as the parking spaces they serve. Changes in level are not permitted.**

*2010 ADAS Section 502.4*

*Parking spaces and access aisles serving them shall comply with 302. Access aisles shall be at the same level as the parking spaces they serve. Changes in level are not permitted.*

*2010 ADAS Section 302.1*

*Floor and ground surfaces shall be stable, firm, and slip resistant and shall comply with 302.*

#### **Citation:**

**2010 ADAS Section: 502.4, 302.1**

#### **As Built:**

**There is considerable debris and dirt build up at the head of the parking spaces and access aisles.**

#### **Recommendation:**

**Install a curb at the head of the accessible parking spaces and access aisles to reduce debris and dirt.**

#### **Barrier Priority:**

**1 (High): Should be completed immediately. (Includes findings that have little or no cost, were in violation of the codes at the time of construction, or pose an imminent safety threat)**



**Finding #3 Continued**



## Accessible Routes

Lat: 38.9312782000, Long: -104.8158318000

### Finding: 4

There are multiple issues with this curb ramp including:

There is 14% running slope at the gutter.

There is a 3/4" lip between the curb ramp and gutter.

There is no level landing, as the upper landing is 4%.

There are no detectable warnings.

2010 ADAS Section 406  
Curb Ramps

### Citation:

2010 ADAS Section: 406

### As Built:

There are multiple issues with the curb ramp at Mark Dabling Blvd and Corporate Dr.

### Recommendation:

Remove and replace the curb ramp.

### Barrier Priority:

1 (High): Should be completed immediately. (Includes findings that have little or no cost, were in violation of the codes at the time of construction, or pose an imminent safety threat)



Image of noncompliant curb ramp

Finding #4 Additional Finding Photos



Finding 187196 Additional Photo

CORRECTED



## Accessible Routes

**Lat: 38.9312782000, Long: -104.8158318000**

### Finding: 5

The cross slope of the pedestrian access route is excessive.

The cross slope of pedestrian access routes shall be 2 percent maximum.

2011 PROWAG Section R302.6

Except as provided in R302.6.1 and R302.6.2 (Pedestrian Street Crossings Without Yield or Stop Control and Midblock Pedestrian Street Crossings), the cross slope of pedestrian access routes shall be 2 percent maximum.

Advisory R302.6 Cross Slope. The cross slope requirements in R302.6 apply to sidewalks and other pedestrian circulation paths, pedestrian street crossings and at-grade rail crossings, and pedestrian overpasses and underpasses and similar structures (see R302.2). The cross slope of the pedestrian access route is measured perpendicular to the direction of pedestrian travel. Cross slope requirements are contained in R304.5.3 for curb ramps and blended transitions, and in R407.3 for ramps.

#### Citation:

2011 PROWAG Section: R302.6

#### As Built:

The sidewalk along Mark Dabling Blvd has a cross slope of 3%.

#### Recommendation:

Remove and replace the sidewalk.

#### Barrier Priority:

1 (High): Should be completed immediately. (Includes findings that have little or no cost, were in violation of the codes at the time of construction, or pose an imminent safety threat)



Image of sidewalk with excessive cross slopes

Finding #5 Additional Finding Photos



Finding 187197 Additional Photo

CORRECTED

## Accessible Routes

Lat: 38.9312782000, Long: -104.8158318000

### Finding: 6

The surface of the pedestrian access route has vertical discontinuities greater than 1/2 inch or between 1/4 inch and 1/2 inch that are not beveled.

2011 PROWAG Section R302.7.2

Vertical surface discontinuities shall be 13 mm (0.5 in) maximum. Vertical surface discontinuities between 6.4 mm (0.25 in) and 13 mm (0.5 in) shall be beveled with a slope not steeper than 50 percent. The bevel shall be applied across the entire vertical surface discontinuity. Advisory R302.7.2 Vertical Surface Discontinuities. The allowance for vertical surface discontinuities is for occasional expansion joints and objects such as utility covers, vault frames, and gratings that cannot be located in another portion of the sidewalk outside the pedestrian access route. However, objects such as utility covers, vault frames, and gratings should not be located on curb ramp runs, blended transitions, turning spaces, or gutter areas within the pedestrian access route. This may not always be possible in alterations, but should be avoided wherever possible. Vertical surface discontinuities between unit pavers should be minimized.

#### Citation:

2011 PROWAG Section: R302.7.2

#### As Built:

There are two areas of 1" vertical displacement on the bridge.

#### Recommendation:

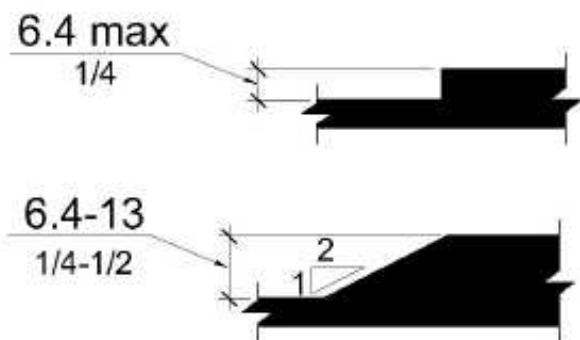
Grind to less than 1/2".

#### Barrier Priority:

1 (High): Should be completed immediately. (Includes findings that have little or no cost, were in violation of the codes at the time of construction, or pose an imminent safety threat)



Image of sidewalk with vertical displacement



**Finding #6 Additional Finding Photos**



Additional image of vertical displacement